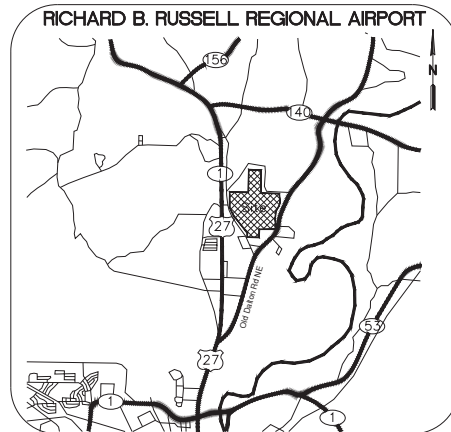


CONSTRUCTION PLANS FOR

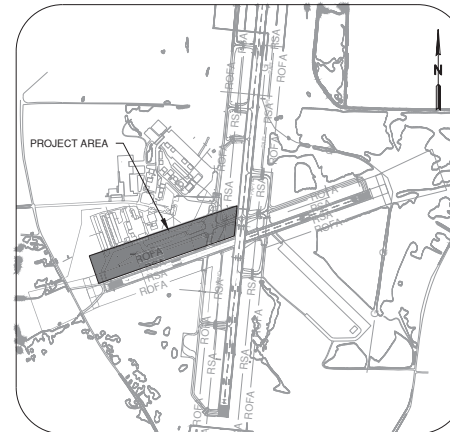


RICHARD B. RUSSELL REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

TAXIWAY BRAVO WEST RELOCATION
JUNE, 2026 - ISSUED FOR BID
GDOT PROJECT NUMBER - APXXX-XXXX-XX(XXX) FLOYD



LOCATION MAP



SITE PLAN

GEORGIA811 COMMON GROUND ALLIANCE
www.call811.com or Phone: 811
Contact 811 when you dig

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 811 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

GEOMETRIC CRITERIA

AIRCRAFT APPROACH CATEGORY (AAC):	B
AIRPLANE DESIGN GROUP (ADG):	II
TAXIWAY DESIGN GROUP (TDG):	II
TAXIWAY SAFETY AREA (TSA):	75'
TAXIWAY OBJECT FREE AREA (TOFA):	131'
RUNWAY SAFETY AREA (RSA):	
RUNWAY 1 - 19:	500'
RUNWAY 7 - 25:	150'
RUNWAY OBJECT FREE AREA (ROFA):	
RUNWAY 1 - 19:	800'
RUNWAY 7 - 25:	500'



- ATLANTA, GA ■ AURORA, IL ■ CHICAGO, IL
- COLUMBUS, OH ■ EDWARDSVILLE, IL
- INDIANAPOLIS, IN ■ KANSAS CITY, MO
- NASHVILLE, TN ■ PEORIA, IL ■ ROCKFORD, IL
- SPRINGFIELD, IL ■ SPRINGFIELD, MO
- ST. LOUIS, MO

SUBMITTED BY _____
DATE _____

CMT JOB NUMBER: 22004559-00

THE FOLLOWING NOTES ARE IN ACCORDANCE WITH FAA AC 5370-2G, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.

SAFETY PLAN COMPLIANCE DOCUMENT:

THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DESCRIBING HOW THE REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN WILL BE MET. THE SPCD SHALL INCLUDE A CERTIFICATION STATEMENT BY THE CONTRACTOR THAT: 1) INDICATES FULL UNDERSTANDING OF THE OPERATIONAL SAFETY REQUIREMENTS OF THE CONSTRUCTION SAFETY AND PHASING PLAN AND 2) ASSERTS NO DEVIATIONS SHALL BE MADE FROM THE APPROVED SAFETY PHASING PLAN AND SAFETY PLAN APPROVAL IS GRANTED BY THE AIRPORT OPERATOR. THE SPCD SHALL FOLLOW THE FORMAT OF FAA AC 150/5370-2G, SECTION 204(b) AND SHALL ADDRESS ITEMS (1)-(18) PER THE REQUIREMENTS OF THE ACD.

THE CONTRACTOR SHALL SUBMIT THE SPCD TO THE PROJECT ENGINEER FOR REVIEW AND APPROVAL BY THE AIRPORT SPONSOR FOURTEEN (14) DAYS PRIOR TO THE ANTICIPATED ISSUE OF THE NOTICE TO PROCEED. COPIES OF THE APPROVED CONSTRUCTION SAFETY PHASING PLAN AND SAFETY PLAN COMPLIANCE DOCUMENT SHALL BE MAINTAINED AT THE PROJECT SITE.

GENERAL CONSIDERATIONS:

AIRPORT OPERATORS CONDUCTING CONSTRUCTION ON LEASED PROPERTIES, SHALL USE THE PRE-DESIGN, PRE-BID, AND PRE-CONSTRUCTION MEETINGS TO OBTAIN CLARIFICATION OF SAFETY DURING CONSTRUCTION. COORDINATE THE FOLLOWING AS REQUIRED:

A. OPERATIONAL SAFETY SHALL BE A STANDING AGENDA ITEM DURING EVERY PROGRESS MEETING.

B. CHANGES IN THE SCOPE OR DURATION OF THE PROJECT MAY REQUIRE REVISIONS TO THE CONSTRUCTION SAFETY PHASING PLAN (AND REVIEW AND APPROVAL BY THE AIRPORT OPERATOR AND FAA).

C. EARLY COORDINATION WITH FAA ATO IS REQUIRED TO SCHEDULE AIRWAY FACILITY SHUTDOWNS AND RESTARTS. RELOCATION OR ADJUSTMENTS TO NAVAIDS OR THEIR CRITICAL AREAS MAY REQUIRE A FAA FLIGHT INSPECTION PRIOR TO RESTARTING THE FACILITY.

AREAS AND OPERATIONS AFFECTED BY CONSTRUCTION ACTIVITY:

SEE THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) FOR A DEPICTION OF THE CONSTRUCTION. THIS DRAWING INCLUDES AFFECTED AREAS AND THE DESCRIPTION OF MITIGATION FOR CONSTRUCTION PHASES.

NAVAIDS:

A. IF THIS PROJECT AFFECTS FAA OWNED AND OPERATED RUNWAY NAVAIDS, COORDINATION WITH LOCAL TECHNICAL OPERATIONS WILL BEGIN AT THE PRE-CONSTRUCTION MEETING. A LIST OF RESPONSIBLE PARTY REPRESENTATIVES, INCLUDING PROCEDURES FOR CONTACT AFTER-HOURS, SHALL BE PROVIDED TO THE CONTRACTOR PRIOR TO CONSTRUCTION. A 45-DAY MINIMUM NOTICE SHALL BE PROVIDED TO FAA ATO/TECH OPS REGARDING SHUT-DOWN OF A NAVAID MORE THAN 24 HOURS OR MORE THAN 4 HOURS ON CONSECUTIVE DAYS. ALL COORDINATION WITH THE FAA ATO/TECH OPS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR

B. BEFORE COMMENCING CONSTRUCTION ACTIVITY, PARKING VEHICLES, OR STORING CONSTRUCTION EQUIPMENT AND MATERIALS NEAR A NAVAID, COORDINATE WITH THE APPROPRIATE FAA ATO/TECH OPS OFFICE TO EVALUATE THE EFFECT OF CONSTRUCTION ACTIVITY AND THE REQUIRED DISTANCE AND DIRECTION FROM THE NAVAID. SEE THE CSPP FOR APPLICABLE NAVAID CRITICAL AREAS. ALL COORDINATION WITH THE FAA ATO/TECH OPS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR

C. INTERFERENCE FROM CONSTRUCTION EQUIPMENT AND ACTIVITIES MAY REQUIRE NAVAID SHUTDOWN OR ADJUSTMENT OF INSTRUMENT APPROACH MINIMUMS FOR LOW VISIBILITY OPERATIONS. THIS REQUIRES THAT A NOTAM BE FILED.

CONTRACTOR ACCESS:

A. THE CONTRACTOR SHALL PROVIDE A GATE GUARD AT ALL POINTS OF ENTRY USED BY THE CONTRACTOR. GATES MUST BE LOCKED WHEN NOT IN USE OR NOT GUARDED BY THE CONTRACTOR. THE CONTRACTOR SHALL TAKE CARE TO MAINTAIN A HIGH LEVEL OF SAFETY AND SECURITY DURING CONSTRUCTION WHEN ACCESS POINTS ARE CREATED IN THE SECURITY FENCING TO PERMIT THE PASSAGE OF CONSTRUCTION VEHICLES OR PERSONNEL. TEMPORARY GATES SHOULD BE EQUIPPED SO THEY CAN BE SECURELY CLOSED AND LOCKED TO PREVENT ACCESS BY ANIMALS AND PEOPLE. PROCEDURES SHOULD BE IN PLACE TO ENSURE THAT ONLY AUTHORIZED PERSONS AND VEHICLES HAVE ACCESS TO THE AOA, AND TO PROHIBIT PIGGYBACKING BEHIND ANOTHER VEHICLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER OPERATION AND PRESERVATION OF ALL AIRFIELD PERIMETER GATES, EITHER MANUAL OR AUTOMATED, FOR THE PURPOSES OF THIS CONTRACT. ANY DAMAGE TO FENCING/GATES AS A RESULT OF CONSTRUCTION TRAFFIC SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

B. STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE SAFETY AREA OR OBJECT FREE AREA (OFA) OF AN OPERATIONAL RUNWAY OR TAXIWAY (OFA STOCKPILES REQUIRE FAA APPROVAL). IF THE CONTRACTOR WISHES TO STOCKPILE MATERIALS OR EQUIPMENT ADJACENT TO AN OFA, HE MUST FIRST COORDINATE WITH THE AIRPORT OPERATOR TO ENSURE THAT 1) APPROPRIATE LIGHTING AND BARRICADES ARE IN PLACE, AND 2) THE STOCKPILED MATERIALS DO NOT CREATE A WILDLIFE ATTRACTANT OR FOREIGN OBJECT DEBRIS (FOD) HAZARD. EXCAVATION AND RESULTANT STOCKPILES ADJACENT TO OTHER PAVED SURFACES MUST BE APPROPRIATELY MARKED WITH BARRICADES, AS DIRECTED BY THE AIRPORT OPERATOR.

C. THE CONTRACTOR'S EQUIPMENT IS STRICTLY LIMITED TO THE CONSTRUCTION AREAS DURING CONSTRUCTION AND TO THE CONTRACTOR STORAGE AREA DURING NON-CONSTRUCTION PERIODS, EXCEPT AS PROVIDED ON THE PLANS, IN THE CONTRACT SPECIAL PROVISIONS, OR AS MAY BE AUTHORIZED BY THE AIRPORT OPERATOR IN WRITING. CONTRACTOR PARKING SHALL BE LIMITED TO THE STAGING AREA, EXCEPT AS ALLOWED BY THE AIRPORT.

D. HAUL ROUTES AND ACCESS TO THE CONSTRUCTION SITE(S) WILL BE AS SHOWN ON THE CONSTRUCTION PHASING SHEET AND DISCUSSED AT THE PRE-BID MEETING. THE CONTRACTOR IS RESPONSIBLE FOR RESTORING ALL HAUL ROADS, PLANT SITES, STAGING AND STORAGE AREAS TO ORIGINAL CONDITION OR TO THE SATISFACTION OF THE AIRPORT. THIS WILL INCLUDE BUT NOT BE LIMITED TO GRADING/FILLING IF NECESSARY, AND SEEDING AND MULCHING ALL TURF AREAS. ANY PAVEMENT AREAS USED AS A HAUL ROUTE WHICH ARE DAMAGED WILL BE REPAVED AS APPROVED BY THE AIRPORT AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL INSPECT ALL HAUL ROUTES PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR DOCUMENTATION OF EXISTING HAUL ROUTES, EITHER WITH PICTURES OR VIDEO.

E. CONSTRUCTION EQUIPMENT: CONSTRUCTION EQUIPMENT MUST BE MARKED WITH A 3 FEET BY 3 FEET ORANGE AND WHITE CHECKERED FLAG AND AMBER BEACON. FOR NIGHT CONSTRUCTION, EQUIPMENT MUST BE EQUIPPED WITH AN AMBER BEACON. CONSTRUCTION VEHICLES MUST BE CLEARLY MARKED WITH THE COMPANY NAME/LOGO AT ALL TIMES. MARKINGS AND LIGHTING SHALL OTHERWISE BE IN ACCORDANCE WITH AC 150/5210-5.

F. AIRPORT RADIOS, FLAG PERSON AND DRIVER TRAINING: THE CONTRACTOR SHALL MAINTAIN AND MONITOR AN AIRPORT RADIO ON-SITE. THE RADIO FREQUENCY FOR THE AIRPORT IS 122.725. RADIOS ARE NOT AVAILABLE FROM THE AIRPORT. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AN ADEQUATE NUMBER OF RADIOS FOR THEIR OPERATIONS. TRAINING OF CONTRACTORS ON PROPER COMMUNICATION PROCEDURES IS ESSENTIAL FOR MAINTAINING AIRPORT OPERATIONAL SAFETY. IF THE AIRPORT PROVIDES RADIO COMMUNICATION TRAINING, CONTRACTOR ATTENDANCE WILL BE REQUIRED. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL BE REQUIRED TO HAVE A TRAINED FLAG PERSON EQUIPPED WITH AN AIRPORT RADIO OPERATING ON 119.0 MHz TO ESCORT ANY CONTRACTOR VEHICLES WHEN CROSSING ACTIVE AOAS. KEY CONTRACTOR PERSONNEL MAY BE EXPECTED TO TRAVEL ON OR NEAR ACTIVE AOAS. THESE INDIVIDUALS MUST COMPLY WITH THE AIRPORT OPERATOR'S RULES AND REGULATIONS BY PARTICIPATING IN ANY APPLICABLE AIRFIELD DRIVERS TRAINING PROCEDURES PROVIDED BY THE AIRPORT.

G. MAINTENANCE OF THE SECURED AREA: SEE ITEM (A) ABOVE.

WILDLIFE MANAGEMENT:

A. THE CSPP AND SPCD MUST BE IN COMPLIANCE WITH THE AIRPORT'S WILDLIFE MANAGEMENT PLAN, IF APPLICABLE.

B. CONTRACTORS MUST CAREFULLY CONTROL AND CONTINUOUSLY REMOVE WASTE OR LOOSE MATERIALS THAT CAN CREATE WILDLIFE HAZARDS OR ATTRACTANTS. THESE INCLUDE TRASH, STANDING WATER, TALL GRASS, SCATTERED SEEDS, POORLY MAINTAINED FENCING, AND DISRUPTION OF EXISTING HABITAT.

FOREIGN OBJECT DEBRIS MANAGEMENT:

A. DEBRIS SHALL NOT BE DEPOSITED ON ANY PORTION OF AN OPERATIONAL RUNWAY, TAXIWAY OR APRON. SHOULD ANY DEBRIS BE ACCIDENTALLY DEPOSITED ON ACTIVE OPERATIONAL AREAS, IT SHALL BE REMOVED IMMEDIATELY. THE CONTRACTOR SHALL HAVE SWEEPING AND/OR VACUUMING CAPABILITIES ON-SITE IN ORDER TO CONTINUOUSLY REMOVE DEBRIS FROM ACTIVE OPERATIONAL AREAS DURING THE CONSTRUCTION PROJECT. PRIOR TO OPENING AIRCRAFT MOVEMENT AREAS CLOSED FOR THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS CLEAR OF ANY FOREIGN OBJECT DEBRIS (FOD) AND IS ACCEPTABLE TO THE AIRPORT. THE CONTRACTOR SHALL CONDUCT DAILY INSPECTIONS OF THE WORK AND ADJACENT AREAS FOR SAFETY AND CLEANLINESS. THE AIRPORT MAY ALSO PERFORM DAILY INSPECTIONS. UPON COMPLETION OF THIS PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE, INCLUDING ADJACENT PAVEMENTS AND HAUL ROUTES, IS RETURNED TO ORIGINAL CONDITION.

B. CONTRACTOR SHALL USE ALL MEANS NECESSARY TO MINIMIZE DUST DURING CONSTRUCTION OPERATIONS. IF THE AIRPORT OR ENGINEER REQUESTS DUST CONTROL IT SHALL BE APPLIED IMMEDIATELY.

HAZARDOUS MATERIALS MANAGEMENT:

A. ANY TYPE OF FUELING SUPPORT FACILITY OR DEVICE USED TO REFUEL CONSTRUCTION EQUIPMENT IS SUBJECT TO LOCAL FIRE INSPECTION. LOCAL FIRE CODES AND SAFETY STANDARDS SHALL BE MET PRIOR TO COMMENCEMENT OF WORK. FUELING TO ONLY BE DONE IN STAGING AREAS.

B. CONTRACTORS OPERATING CONSTRUCTION VEHICLES AND EQUIPMENT ON THE AIRPORT MUST BE PREPARED TO EXPEDITIOUSLY CONTAIN AND CLEAN UP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS.

C. TRANSPORT AND HANDLING OF OTHER HAZARDOUS MATERIALS ALSO REQUIRE SPECIAL PROCEDURES (SEE AC 150/5320-15, MANAGEMENT OF AIRPORT INDUSTRIAL WASTE).

NOTIFICATION OF CONSTRUCTION ACTIVITIES:

A. THE CONTRACTOR SHALL INCLUDE IN THE SPCD A PROCEDURE FOR IMMEDIATE NOTIFICATION OF THE AIRPORT AND LOCAL FAA OF ANY ADVERSE CONDITIONS AFFECTING SAFETY ON THE AIRPORT. THE CONTRACTOR SHALL OBTAIN CONTACT INFORMATION FOR ALL INVOLVED PARTIES AND PROCEDURES FOR CONTACTING THEM 24 HOURS A DAY, 7 DAYS A WEEK.

B. THE AIRPORT SHALL ISSUE ALL NOTAMS RELEVANT TO THE CONSTRUCTION PROJECT. THE CONTRACTOR SHALL PROVIDE INFORMATION ON CLOSED OR HAZARDOUS CONDITIONS TO THE OCC.

C. THE CONTRACTOR SHALL PREPARE AND SUBMIT CONTACT INFORMATION FOR LOCAL MEDICAL, FIRE FIGHTING, AND POLICE RESPONSE IN CASE OF EMERGENCIES DURING CONSTRUCTION.

D. THE CONTRACTOR SHALL BE REQUIRED TO COORDINATE WITH AIRPORT AFFF PERSONNEL, (IF APPLICABLE) EVEN FOR NON-EMERGENCIES.

INSPECTION REQUIREMENTS:

A. AIRPORT SAFETY SELF-INSPECTIONS SHOULD BE CONDUCTED BY THE CONTRACTOR AT LEAST DAILY, BUT MORE FREQUENTLY IF NECESSARY TO CONFORM WITH THE CSPP. A SAMPLE DAILY INSPECTION CHECKLIST IS AVAILABLE IN APPENDIX 4 OF AC 150/5370-2G.

B. CONTRACTOR SHALL ATTEND THE FINAL INSPECTION MEETING THAT WILL BE HELD FOR THIS PROJECT.

UNDERGROUND UTILITIES:

A. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF EXISTING AIRPORT UTILITIES, AND ELECTRICAL CIRCUITS, WHETHER OWNED BY THE AIRPORT OR OTHER AGENCIES OPERATING AT THE AIRPORT (SEE GENERAL PROVISION 70-15). LOCATIONS OF EXISTING CABLE SHOWN IN THE PLANS ARE BASED ON AVAILABLE RECORD DATA AND ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL COORDINATE THE LOCATION OF ANY CROSSING OF UNDERGROUND ELECTRICAL CIRCUITS WITH THE AIRPORT AND FAA FACILITIES PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL PROVIDE TONING EQUIPMENT CAPABLE OF DETECTING A 60 Hz SIGNAL FOR LOCATING CABLES IN THE CONSTRUCTION AREA. WORKING IN CONJUNCTION WITH THE AIRPORT, THE CONTRACTOR SHALL LOCATE EXISTING CABLES PRIOR TO START OF CONSTRUCTION. ANY ELECTRICAL CIRCUITS TO BE GROSSED SHALL THEN BE TESTED FOR RESISTANCE TO GROUND BY THE CONTRACTOR UNDER SUPERVISION OF THE ENGINEER AND THE READINGS RECORDED. FOLLOWING CONSTRUCTION, THE CONTRACTOR, UNDER THE SUPERVISION OF THE ENGINEER, SHALL CHECK ALL CIRCUITS GROSSED DURING THE PROJECT. ANY CIRCUITS SHOWING A DECREASE IN RESISTANCE TO GROUND SHALL BE CORRECTED BY THE CONTRACTOR SO AS TO PROVIDE AT LEAST 50 MEGOHMS RESISTANCE TO THAT CIRCUIT. NO ADDITIONAL COMPENSATION WILL BE MADE FOR ANY CORRECTIVE ELECTRICAL WORK DUE TO CONTRACTOR'S OPERATIONS. SEE ELECTRICAL CABLE SPECIFICATIONS FOR REPAIR AND SPlicing REQUIREMENTS.

B. FAA CABLES WILL BE LOCATED AND MARKED BY FAA PRIOR TO CONSTRUCTION. CONTRACTOR IS TO NOTIFY FAA 30 DAYS IN ADVANCE SO THAT FAA CAN SCHEDULE THE REQUIRED MARKING TO PROTECT CABLES DURING CONSTRUCTION. ANY CABLES DAMAGED DURING CONSTRUCTION WILL BE REPLACED BY CONTRACTOR IMMEDIATELY. CONTRACTOR SHALL PAY ALL COSTS ASSOCIATED WITH THE REPAIR OF DAMAGED CABLES AT NO COST TO OWNER.

PENALTIES:

A. THIS PROJECT INVOLVES WORK NEAR ACTIVE AIRCRAFT OPERATIONAL AREAS. ANY RUNWAY OR TAXIWAY INCUSSIONS OCCURRING DURING THE PROJECT, AS DETERMINED BY THE JUDGEMENT OF THE ENGINEER AND/OR AIRPORT, MAY BE SUBJECT TO FAA FINES IN EXCESS OF \$10,000. FAA DEFINES A RUNWAY INCUSSION AS "ANY OCCURRENCE AT AN AERODROME INVOLVING THE INCORRECT PRESENCE OF AN AIRCRAFT, PERSON OR PERSON ON THE PROTECTED AREA OF A SURFACE DESIGNATED FOR THE LANDING AND TAKE OFF OF AIRCRAFT."

RUNWAY & TAXIWAY VISUAL AIDS-MARKING, LIGHTING, SIGNS, AND VISUAL NAVAIDS:

A. AIRPORT MARKINGS, LIGHTING, SIGNS, AND VISUAL NAVAIDS MUST BE CLEARLY VISIBLE TO PILOTS. ALL MUST BE SECURED IN PLACE TO PREVENT MOVEMENT BY PROP WASH, JET BLAST, WIND VORTICES OR OTHER WIND CURRENTS, BUT FRANGIBLE AND CONSTRUCTED OF MATERIALS THAT WOULD MINIMIZE DAMAGE TO AN AIRCRAFT IN THE EVENT OF INADVERTENT CONTACT.

B. THE FOLLOWING ARE APPLICABLE STANDARDS FOR ANY TEMPORARY OR PERMANENT INSTALLATIONS OF THESE RESPECTIVE ITEMS:
AIRCRAFT MARKINGS: AC 150/5340-1
LIGHTING: AC 150/5340-30, 150/5345-50, 150/5345-53
SIGNAGE: AC 150/5345-44, 150/5340-18, 150/5345-53

C. SHOULD A RUNWAY BE TEMPORARILY CLOSED AND REQUIRE A CLOSURE CROSS, THE CONTRACTOR SHALL MARK THE AFFECTED RUNWAY WITH A CLOSURE CROSS IN ACCORDANCE WITH THE CLOSURE CROSS SPECIFICATIONS FOUND IN THE PLANS. CLOSURE CROSSES SHALL REMAIN IN PLACE UNTIL NORMAL RUNWAY OPERATIONS RESUME. WHEN NECESSARY TO CLOSE, OR CHANGE THE STANDARD OPERATIONS OF A RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL, THROUGH THE ENGINEER, NOTIFY THE AIRPORT 72 BUSINESS HOURS IN ADVANCE OF THE PROPOSED CHANGE IN OPERATIONS.

MARKING AND SIGNS FOR ACCESS ROUTES:

A. PAVEMENT MARKINGS AND SIGNS INTENDED FOR CONSTRUCTION PERSONNEL SHOULD CONFORM TO AC 150/5340-18, AND WITH THE MUTCD AND/OR STATE HIGHWAY SPECIFICATIONS AS PRACTICAL.

HAZARD MARKING AND LIGHTING:

A. IF ANY AIRCRAFT MOVEMENT AREAS MUST BE CLOSED, THE CONTRACTOR SHALL FURNISH AND PLACE PORTABLE BARRICADES ACROSS RUNWAYS OR TAXIWAYS TO KEEP VEHICLES FROM ENTERING ACTIVE OPERATION AREAS AND TO KEEP AIRCRAFT FROM TAXIING INTO CONSTRUCTION AREAS. BARRICADES SHALL BE HIGHLY-REFLECTIVE, MARKED WITH DIAGONAL, ALTERNATING ORANGE AND WHITE STRIPES AND SUPPLEMENTED WITH EITHER FLASHING OR STEADY-BURNING LIGHTS DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS. LIGHTS SHALL BE BARRICADE TYPE TYPICAL FOR CONSTRUCTION ZONES AND RED IN COLOR, MEETING ALL STATE HIGHWAY REQUIREMENTS. BARRICADES LOCATED WITHIN AIRCRAFT MOVEMENT AREAS (RUNWAYS, TAXIWAYS, APRONS) SHALL BE LOW LEVEL AVIATION BARRICADES (NO HIGHER THAN 18") SPECIFICALLY MANUFACTURED AND DESIGNED FOR SUCH PURPOSE. TO PROGRESS, THE AIRPORT OPERATOR SHALL BE ADVISED IN COLOR, HEIGHT AND 96" LONG, MADE OF UV-RESISTANT POLYETHYLENE AS MANUFACTURED BY MULTI-BARRIER (MODEL AR 10x96 HDPE), OR APPROVED EQUAL. ENOUGH BARRICADES MUST BE PROVIDED TO SAFELY DELINEATE ALL CONSTRUCTION BOUNDARIES. BARRICADES SHALL BE PLACED CLOSELY ENOUGH TO PREVENT TRAFFIC FROM ENTERING THE CONSTRUCTION AREA, GENERALLY NO MORE THAN 4 FEET APART. BARRICADES SHALL BE FILLED WITH WATER TO PREVENT MOVEMENT BY JET BLAST. BARRICADES LOCATED OUTSIDE OF AIRCRAFT MOVEMENT AREAS MAY BE MADE FROM VARIOUS MATERIALS, INCLUDING

SAWHORSES, BARRELS, OR OTHER STANDARD HIGHWAY BARRICADES. IF BARRICADES ARE SUBJECT TO PROP WASH, JET BLAST, WIND VORTEX OR ANY OTHER SURFACE WIND CURRENTS, THE BARRICADES WILL BE WEIGHTED OR FASTENED TO THE GROUND TO PREVENT DISPLACEMENT.

B. HAZARDS SUCH AS OPEN MANHOLES, AREAS UNDER REPAIR, STOCKPILE AND WASTE AREAS SHOULD ALSO BE MARKED.

C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MOVEMENT OF BARRICADES BETWEEN PHASES, OR AS OTHERWISE CRITICAL TO AIRPORT OPERATIONS. ALL BARRICADES MUST BE CHECKED NIGHTLY, AND REPLACED BY THE CONTRACTOR IF NOT FUNCTIONING.

PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS:

A. CONSTRUCTION ACTIVITY ADJACENT TO ACTIVE RUNWAYS, TAXIWAYS, AND APRONS SHALL BE COORDINATED WITH AIRPORT OPERATIONS. CONSTRUCTION ACTIVITY IN THESE AREAS WILL BE AUTHORIZED AFTER 1) NOTICES TO AIRMEN (NOTAMS) HAVE BEEN ISSUED BY THE AIRPORT, 2) BARRICADING AND LIGHTING PROVISIONS HAVE BEEN IMPLEMENTED BY THE CONTRACTOR, AND 3) IT HAS BEEN DETERMINED THAT THE HEIGHT OF EQUIPMENT AND MATERIALS IS BEYOND THE REACH, OR SAFELY BELOW, AIRCRAFT USING ADJACENT OPERATION AREAS.

B. NO CONSTRUCTION MAY OCCUR WITHIN AN EXISTING RUNWAY OR TAXIWAY SAFETY AREA WHILE THE RUNWAY/TAXIWAY IS OPEN TO AIRCRAFT OPERATIONS. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE SAFETY AREA OF AN OPEN RUNWAY/TAXIWAY. IF THE RUNWAY/TAXIWAY MUST BE OPENED BEFORE EXCAVATIONS ARE BACKFILLED, THE EXCAVATIONS MUST BE COVERED IN SUCH A WAY AS TO ALLOW THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY/TAXIWAY TO CROSS THE COVERING WITHOUT DAMAGE. THE SAFETY AREA MUST BE GRADED WITH NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS UPON OPENING, OPEN TRENCHES AND EXCAVATIONS WITHIN THE CONSTRUCTION AREA(S) MUST BE PROMINENTLY MARKED.

C. CONSTRUCTION MAY BE PERMITTED IN THE RUNWAY OBJECT FREE AREA, BUT EQUIPMENT MUST BE REMOVED FROM THE ROFA WHEN NOT IN USE AND MATERIAL SHOULD NOT BE STOCKPILED WITHIN THE ROFA. NO CONSTRUCTION MAY TAKE PLACE WITHIN THE OBJECT FREE AREA OF AN OPEN TAXIWAY, UNLESS 1) THE TOFA DIMENSION IS TEMPORARILY ADJUSTED FOR USE BY SMALLER AIRCRAFT ONLY; 2) TEMPORARY OFFSET TAXIWAY MARKINGS ARE USED; OR 3) CONSTRUCTION PROCEEDS WITH THE FOLLOWING RESTRICTIONS: APPROPRIATE NOTICES TO AIRMEN (NOTAMS) HAVE BEEN ISSUED BY THE AIRPORT, BARRICADING AND LIGHTING PROVISIONS HAVE BEEN IMPLEMENTED BY THE CONTRACTOR, AND FLAGGERS AND WINGWALKERS ARE UTILIZED TO MAINTAIN A FIVE-FOOT SEPARATION BETWEEN AIRCRAFT AND ALL EQUIPMENT OR MATERIALS. ANY ADJUSTMENT OF RSA OR TSA DIMENSIONS SHALL BE COORDINATED WITH THE ATCT AND/OR APPROPRIATE FAA REGIONAL/AOD OFFICE.

D. PERSONNEL, MATERIAL, AND/OR EQUIPMENT MAY NOT PENETRATE THE RUNWAY OBSTACLE FREE ZONE (OFZ) OR THRESHOLD SIGHTING SURFACES WHILE THE RUNWAY IS OPEN. RUNWAY APPROACH/DEPARTURE AREAS AND CLEARWAYS MUST BE PROTECTED.

E. BLASTING OPERATIONS: N/A.

OTHER LIMITATIONS ON CONSTRUCTION:

A. THE FOLLOWING ARE PROHIBITED FROM USE ON THE AIRFIELD: TALL EQUIPMENT BEYOND THE MAXIMUM EQUIPMENT HEIGHT SHOWN ON THE CONSTRUCTION SAFETY PHASING PLAN, OPEN-FLAME WELDING/TORCH CUTTING (UNLESS PROPER PRECAUTIONS ARE TAKEN AND AIRPORT APPROVAL IS OBTAINED), ELECTRICAL BLASTING CAPS (WITHIN 1000' OF AIRPORT PROPERTY), AND FLARE POTS.



Crawford, Murphy & Tilly

CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)



ISSUED FOR BID

TAXIWAY BRAVO WEST RELOCATION

OWNER



RICHARD B. RUSSELL REGIONAL AIRPORT FLOYD COUNTY, GEORGIA

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	22004559-00
CAD DWG FILE:	22004559-G-INFO.DWG
DESIGNED BY:	--
DRAWN BY:	--
CHECKED BY:	--
APPROVED BY:	--
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SHEET TITLE

CONSTRUCTION SAFETY PHASING NOTES

G-002

SHEET 2 OF 81

Path: K:\cmt\Bussell\22004559-00_RMD_D_Taxiway\Wdr\Info\Sign\Draw\Sheet\22004559-G-INFO.dwg Date: Tuesday, June 2, 2026 11:37:27 AM



CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (2X/3/4)



ISSUED FOR BID

TAXIWAY BRAVO WEST RELOCATION

OWNER



RICHARD B. RUSSELL REGIONAL AIRPORT FLOYD COUNTY, GEORGIA

MARK DATE DESCRIPTION

CMT PROJECT NO: 22004559-00
CAD DWG FILE: 22004559-G-INFO.DWG
DESIGNED BY: --
DRAWN BY: --
CHECKED BY: --
APPROVED BY: --
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SHEET TITLE
GENERAL NOTES & SHEET LIST

G-003
SHEET 3 OF 81

Sheet List Table with columns: Sheet Index, Sheet Number, Sheet Title. Lists sheets from G-001 to EC-017 including construction safety, geometry, staking, grading, drainage, and erosion control details.

- 3. OTHER LIMITATIONS ON CONSTRUCTION
1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT...
2. THE CONTRACTOR SHALL MAINTAIN ACCESS FOR ARFF EMPLOYEES...
3. ALL EQUIPMENT MUST BE RETURNED TO THE STAGING AREA AT THE END OF EACH WORK DAY...
4. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING AND PROVIDING ALL PERMANENT AND TEMPORARY UTILITY CONNECTIONS TO THE STAGING AREA...
5. CONTRACTOR SHALL MAINTAIN ALL AIRFIELD SAFETY DEVICES SUCH AS STAKED LIMIT LINES...
6. THE CONTRACTOR WILL BE RESPONSIBLE FOR STAKING AND GRADE CONTROL OF ALL ELEMENTS OF THE CONSTRUCTION...
7. BURNING OF DEBRIS WILL NOT BE ALLOWED ON AIRPORT PROPERTY...
8. WHEN THE CONTRACTOR'S OPERATIONS ENCOUNTER WET CONDITIONS ALONG HAUL AND ACCESS ROUTES, THE CONTRACTOR SHALL EXERCISE CARE WITHIN THESE AREAS TO MINIMIZE DISTURBANCE...
9. CONTRACTOR IS SOLELY RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR THE PROJECT PRIOR TO ANY CONSTRUCTION ACTIVITIES.

LIQUIDATED DAMAGES FOR DAILY, NIGHTLY AND WEEKEND CLOSURES

LIQUIDATED DAMAGES SHALL BE ASSESSED PER THE FOLLOWING:

- 1. FOR EACH OCCURRENCE THAT A RUNWAY REMAINS CLOSED PAST THE PLANNED AND AGREED-TO REOPENING TIME FOR WORK THAT REQUIRES NIGHTLY CLOSURES...
2. FOR EACH OCCURRENCE THAT A TAXIWAY REMAINS CLOSED PAST THE PLANNED AND AGREED-TO REOPENING TIME FOR WORK THAT REQUIRES DAILY CLOSURES...
3. PRIOR TO OPENING THE TAXIWAY OR RUNWAY, THE CONTRACTOR SHALL THOROUGHLY SWEEP AND CLEAN THE PAVEMENTS, REMOVE ALL EQUIPMENT, AND VERIFY THAT THERE ARE NO SLOPES GREATER THAN 5% OR DROP OFFS GREATER THAN 3 INCHES INSIDE THE SAFETY AREA IN ACCORDANCE WITH FAA AC 150/5370-2G (CURRENT). AIRPORT OPERATIONS SHALL BE NOTIFIED AND THE TAXIWAY SHALL BE INSPECTED AND APPROVED FOR USE.

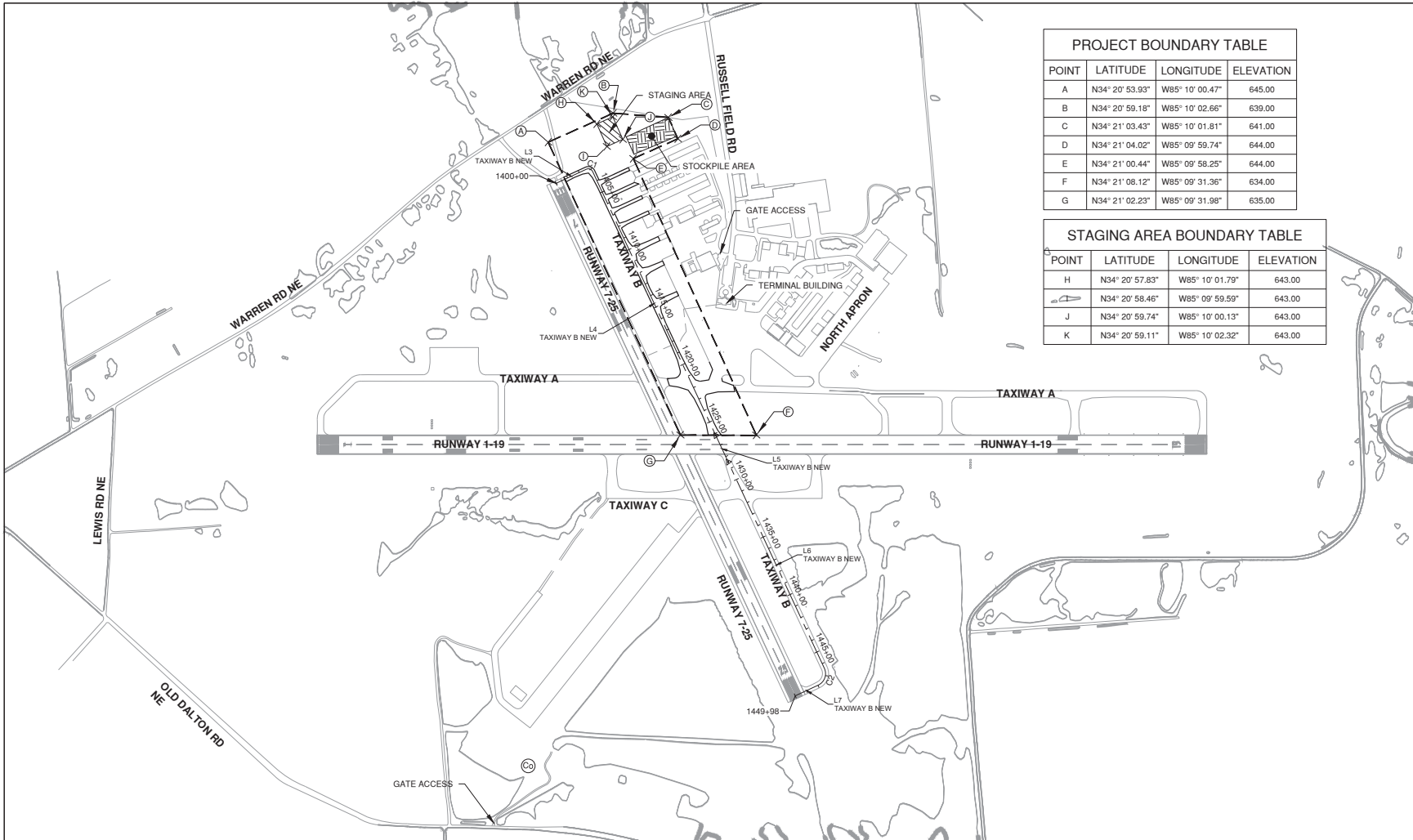
- GENERAL
1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G (CURRENT), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G (CURRENT). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF THE TRAVELING PUBLIC, PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND FAA. THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT BROKEN INTO EACH PHASE AND SUBPHASE. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED EVERY TWO WEEKS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT. THE CONTRACTOR'S SCHEDULE SHALL BE SUBMITTED IN MICROSOFT PROJECT OR SIMILAR ELECTRONIC FORMAT.
3. DURING CONSTRUCTION, THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT OPERATOR AND THE ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT. A TWO (2) WEEK LOOK AHEAD SCHEDULE SHALL BE SUPPLIED AT EACH WEEKLY COORDINATION MEETING.

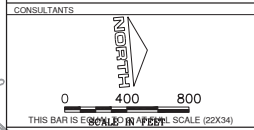
2. SPILL PREVENTION

- 1. ANY LEAKS OR SPILLS OF PETROLEUM PRODUCTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTAIN, CONTROL, REPORT, AND REMEDIATE IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL GUIDELINES, ORDINANCES, AND LAWS.
2. CONTROL OF POLLUTANTS: POLLUTANTS OF POTENTIALLY HAZARDOUS MATERIALS, SUCH AS FUELS, LUBRICANTS, LEAD PAINT, CHEMICALS OR BATTERIES, SHALL BE TRANSPORTED, STORED AND UTILIZED IN A MANNER TO PREVENT LEAKAGE OR SPILLAGE INTO THE ENVIRONMENT. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR PROPER AND LEGAL DISPOSAL OF ALL SUCH MATERIALS. EQUIPMENT, ESPECIALLY CONCRETE OR ASPHALT TRUCKS, SHALL NOT BE WASHED OR CLEANED OUT ON THE PROJECT EXCEPT IN AREAS WHERE UNUSED PRODUCT CONTAMINANTS CAN BE PREVENTED FROM ENTERING WATERWAYS.
3. A SPILL PREVENTION, CONTROL AND COUNTERMEASURES (SPCC) PLAN WILL BE DEVELOPED BY THE CONTRACTOR AND APPROVED PRIOR TO INSTALLATION OF TEMPORARY FUEL TANKS. THIS PLAN WILL REQUIRE THE FOLLOWING:
3.1. ALL BARE SOIL AROUND FUELING AREA LINED TO PREVENT SEEPAGE INTO SOIL. IN ADDITION, A SPILL KIT WILL BE KEPT ON-SITE IN CASE OF LEAKS.
3.2. TERTIARY CONTAINMENT WILL BE PROVIDED IN THE MOBILE TRUCK AREA.
3.3. SECONDARY CONTAINMENT WILL BE PROVIDED IN THE FORM OF A CONTAINMENT PAD WITH A VALVE SIZED (OR APPROVED EQUAL) FOR THE APPROPRIATE REFUELING TRUCK.



PROJECT BOUNDARY TABLE			
POINT	LATITUDE	LONGITUDE	ELEVATION
A	N34° 20' 53.93"	W85° 10' 00.47"	645.00
B	N34° 20' 59.18"	W85° 10' 02.66"	639.00
C	N34° 21' 03.43"	W85° 10' 01.81"	641.00
D	N34° 21' 04.02"	W85° 09' 59.74"	644.00
E	N34° 21' 00.44"	W85° 09' 58.25"	644.00
F	N34° 21' 08.12"	W85° 09' 31.36"	634.00
G	N34° 21' 02.23"	W85° 09' 31.98"	635.00

STAGING AREA BOUNDARY TABLE			
POINT	LATITUDE	LONGITUDE	ELEVATION
H	N34° 20' 57.83"	W85° 10' 01.79"	643.00
I	N34° 20' 58.46"	W85° 09' 59.59"	643.00
J	N34° 20' 59.74"	W85° 10' 00.13"	643.00
K	N34° 20' 59.11"	W85° 10' 02.32"	643.00



ISSUED FOR BID
TAXIWAY BRAVO WEST
RELOCATION



RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

MARK	DATE	DESCRIPTION

CMT PROJECT NO: 22004559-00
CAD DWG FILE: 22004559-G-100.DWG
DESIGNED BY: JBH/JMM
DRAWN BY: JBH
CHECKED BY: JMM
APPROVED BY: MJR
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SHEET TITLE
**PROJECT LAYOUT
PLAN**

LEGEND	
STAGING AREA	
STOCKPILE AREA	
PROJECT BOUNDARY	

- NOTES**
- CONTRACTOR WORK MAY BE RESTRICTED IN CERTAIN AREAS AT VARIOUS TIMES TO BE DETERMINED BY THE OWNER OR RPR.
 - NO WORK CAN BE PERFORMED WITHIN THE RUNWAY RSA OR TAXIWAY TOFA WHILE THE PAVEMENT IS OPEN TO AIRCRAFT OPERATIONS.
 - ALL WORK AREA LIMITS ARE APPROXIMATE. CONTRACTOR SHALL COORDINATE WITH THE OWNER AND ENGINEER AS TO THE ACTUAL LIMITS AND BARRICADE LOCATIONS SO AS TO ALLOW MAXIMUM UTILIZATION OF WORK AREA WHILE ALLOWING FOR CONTINUED AIRCRAFT OPERATIONS.
 - BARRICADES AND TEMPORARY RUNWAY CLOSURE MARKERS SHALL BE SET PRIOR TO BEGINNING WORK WITHIN THE RUNWAY SAFETY AREA AND REMOVED UPON COMPLETION OF THE DAY OR ACTIVITY.
 - CONTRACTOR SHALL PROVIDE A GATE GUARD TO SECURE ACCESS TO AIRFIELD AT DESIGNATED ACCESS GATES DURING PERIODS OF CONTRACTOR ACTIVITY. DURING PERIODS OF INACTIVITY, THE CONTRACTOR SHALL ENSURE THE ACCESS GATES ARE SECURED AND LOCKED. PAYMENT FOR GATE GUARD SHALL BE INCIDENTAL TO MOBILIZATION.
 - CONTRACTOR SHALL FIELD VERIFY THE CONDITION OF THE PROPOSED HAUL ROUTE. ANY DAMAGE TO THE HAUL ROUTE, CAUSED BY THE CONTRACTOR'S OPERATIONS, SHALL BE REPAIRED BY THE CONTRACTOR. COST OF THIS REPAIR SHALL BE INCIDENTAL TO THE BID ITEM FOR MOBILIZATION.
 - ALL CONTRACTORS AND SUBCONTRACTORS SHALL BE ESCORTED BY AUTHORIZED PERSONNEL WHILE IN AIR OPERATIONS AREA AT ALL TIMES.
 - CONTRACTOR SHALL YIELD TO ALL AIRCRAFT AT ALL TIMES.**
 - THE CONTRACTOR SHALL COORDINATE WITH AIRPORT ON SCHEDULING OF PHASES/SUBPHASES TO MINIMIZE IMPACT TO TENANTS AND ALLOW TRAVEL LANES FOR AIRCRAFT UNTIL WORK REQUIRES CLOSURE. CONTRACTOR SHALL NOT WORK ON PHASE/SUBPHASE WITHOUT PRIOR APPROVAL FROM AIRPORT.
 - THE CONTRACTOR SHALL HAUL MILLINGS TO DESIGNATED AREA FOR COUNTY USE.

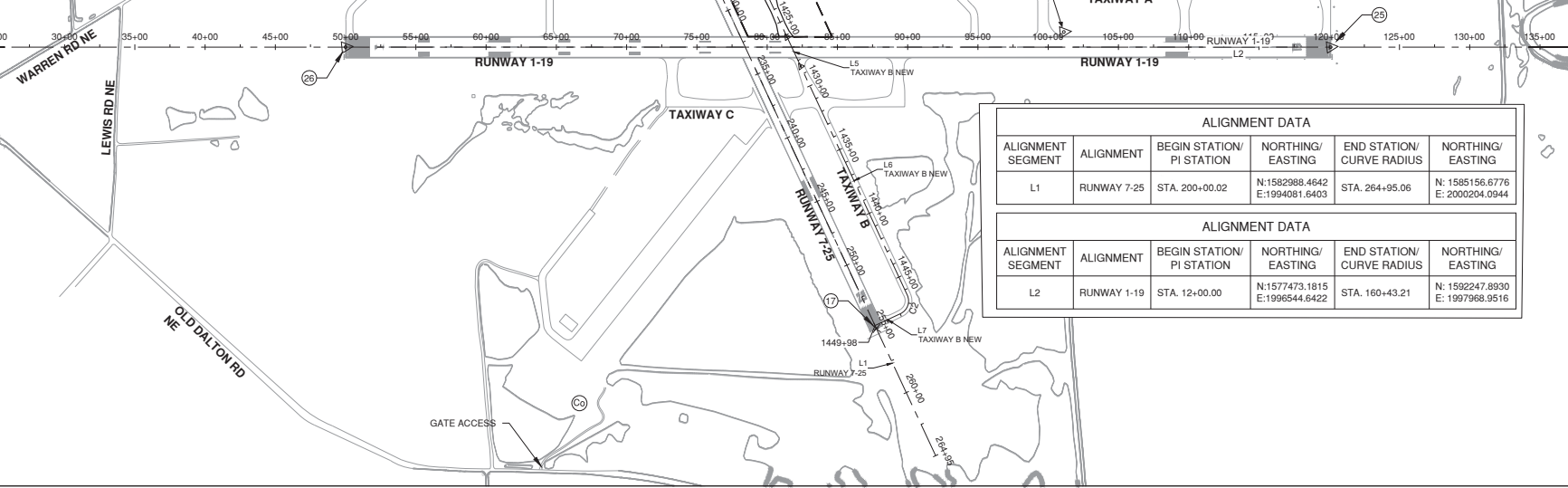
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CONTROL POINT TABLE				
POINT	POINT NUMBER	NORTHING	EASTING	ELEVATION
1" BRASS DISC	26	1581255.636	1996909.270	632.40
1" BRASS DISC	25	1588233.301	1997581.935	622.10
1" BRASS DISC	20	1583360.628	1995132.334	643.50
1" BRASS DISC	17	1584823.681	1999283.802	625.90
1" BRASS DISC	16	1583322.283	1995024.255	644.00
BM MAGNAIL	9	1583678.299	1994991.417	641.97
BRASS DISC IN CONCRETE	23	1585940.185	1996837.574	626.80
CITY OF ROME SURVEY MONUMENT DISC 54	21	1584640.143	1995973.076	634.42
CORNER OF CONCRETE SLAB	22	1585514.894	1995887.071	631.03
MAGNAIL	18	1583553.646	1994949.121	642.27
MAGNAIL	10	1583818.397	1995040.610	641.25
NAIL	15	1583992.392	1995693.734	638.47
NAIL	14	1584187.910	1996344.018	635.17
NAIL IN GRASS	24	1586353.873	1997292.710	624.04
TPNAIL	13	1584313.220	1996640.757	634.11
TPNAIL	12	1584105.221	1995971.542	637.11
TPNAIL	11	1583971.459	1995352.753	640.07

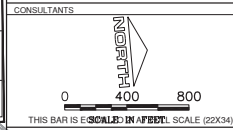
ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L3	TAXIWAY BRAVO NEW	STA. 1400+00.00	N:1583328.1296 E:1995040.7643	STA. 1402+63.00	N:1583576.0425 E:1994952.9681
C1	TAXIWAY BRAVO NEW	CENTER STA. 1403+00.00	PI N:1583610.9201 PI E:1994940.6163	R = 37.000	CENTER N:1583588.3942 E:1994987.8454
L4	TAXIWAY BRAVO NEW	STA. 1403+21.12	N:1583623.2716 E:1994975.4941	STA. 1426+16.98	N:1584389.6887 E:1997139.6508
L5	TAXIWAY BRAVO NEW	STA. 1426+16.98	N:1584389.6887 E:1997139.6508	STA. 1428+47.34	N:1584466.8790 E:1997356.6996
L6	TAXIWAY BRAVO NEW	STA. 1428+47.34	N:1584466.8790 E:1997356.6996	STA. 1446+40.85	N:1585065.1619 E:1999047.4745
C2	TAXIWAY BRAVO NEW	CENTER STA. 1447+40.85	PI N:1585098.4625 PI E:1999141.7669	R = 100.000	CENTER N:1584970.8694 E:1999080.7751
L7	TAXIWAY BRAVO NEW	STA. 1447+97.93	N:1585004.1701 E:1999175.0676	STA. 1449+97.69	N:1584815.8433 E:1999241.6695

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	RUNWAY 7-25	STA. 200+00.02	N:1582988.4642 E:1994081.6403	STA. 264+95.06	N:1585156.6776 E:2000204.0944

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L2	RUNWAY 1-19	STA. 12+00.00	N:1577473.1815 E:1996544.6422	STA. 160+43.21	N:1592247.8930 E:1997968.9516



LEGEND	
STAGING AREA	
STOCKPILE AREA	
PROJECT BOUNDARY	



ISSUED FOR BID

TAXIWAY BRAVO WEST RELOCATION



RICHARD B. RUSSELL REGIONAL AIRPORT FLOYD COUNTY, GEORGIA

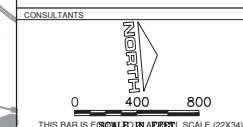
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CAD DWG FILE:	22004559-G-100.DWG
DESIGNED BY:	JBH/JMM
DRAWN BY:	JBH
CHECKED BY:	JMM
APPROVED BY:	MJR
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SHEET TITLE
ALIGNMENT DATA AND BENCHMARK PLAN

G-102
SHEET 6 OF 81

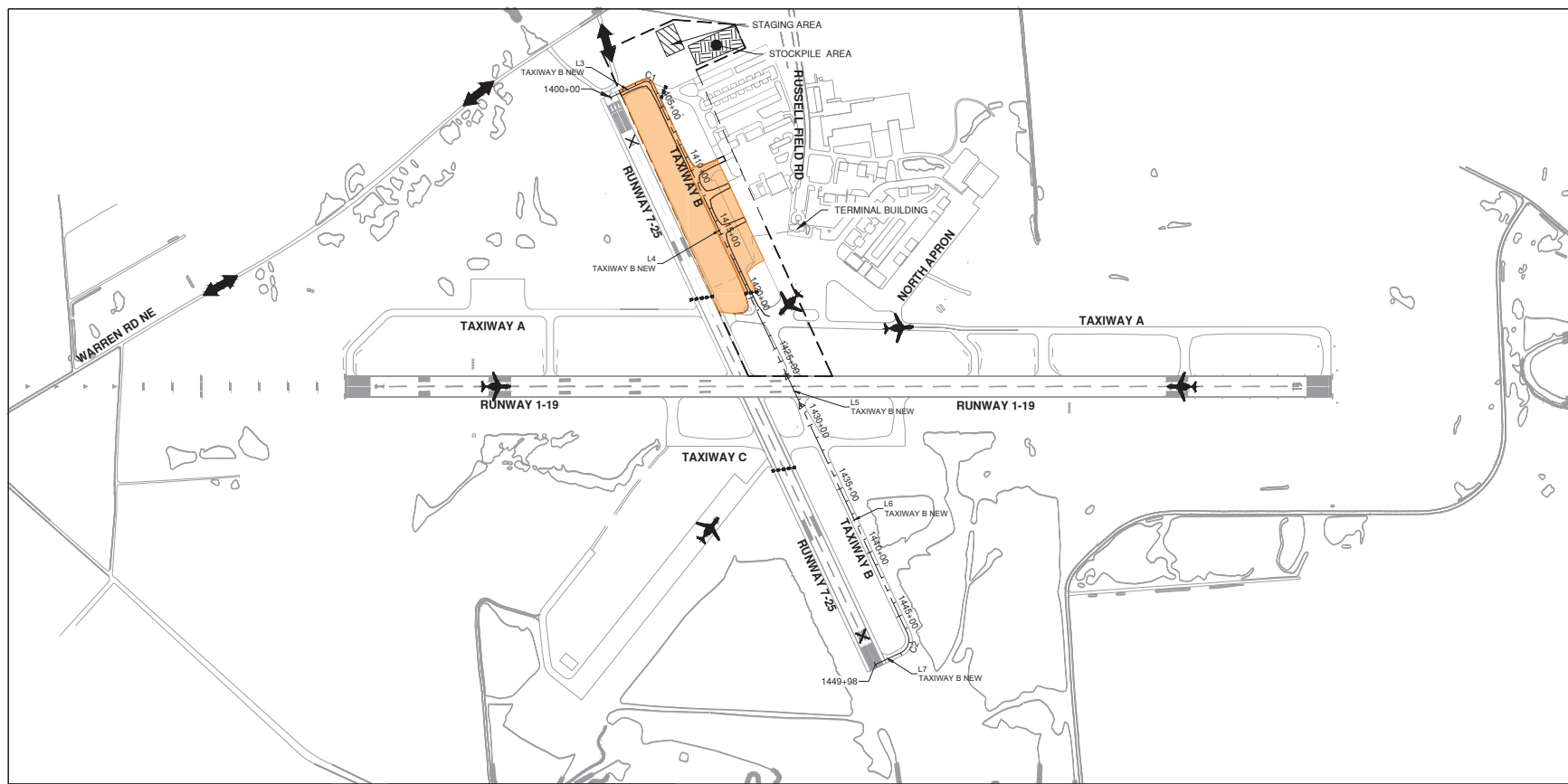
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ISSUED FOR BID
TAXIWAY BRAVO WEST
RELOCATION



RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA



LEGEND	
STAGING AREA	
STOCKPILE AREA	
PROJECT BOUNDARY	
BARRICADE	
RUNWAY CLOSURE MARKER	
AIRCRAFT ACCESS AREA	

BARRICADE SCHEDULE	
LOCATION	# OF BARRICADES REQUIRED FOR CONTINUOUS SECTION
RUNWAY 7-25	17
RUNWAY 1-19	25
TAXIWAY A	8
TAXIWAY B	6
TAXILANE	3

PHASE	CONSTRUCTION	AFFECTED OPERATIONS AREAS	WORK HOURS	SAFETY AND SECURITY	CONSTRUCTION TIME (DAYS)
1	MOBILIZATION, EROSION CONTROL SETUP, TAXIWAY BRAVO WEST PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, LIGHTING INSTALLATION.	RUNWAY 7-25 CLOSED RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	170
2	TAXIWAY ALPHA AND BRAVO INTERSECTION PAVEMENT REMOVAL, GRADING, LIGHTING INSTALLATION & REMOVAL, AND TEMPORARY MARKING.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	30
3	TAXIWAY BRAVO PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, LIGHTING INSTALLATION & REMOVAL, AND TEMPORARY MARKING WITHIN RUNWAY 1-19 RSA.	RUNWAY 7-25 OPEN RUNWAY 1-19 CLOSED	NORMAL / UNRESTRICTED INCLUDING THE WEEKEND	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	10
4	TAXILANE CONNECTORS TO BRAVO WEST PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, AND TEMPORARY MARKING.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	15
5	FULL RATE PAVEMENT MARKING (30 DAYS AFTER ASPHALT PAVING), FINAL GRASSING, REMOVE TEMPORARY EROSION CONTROL DEMOBILIZATION.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	15
TOTAL CONSTRUCTION TIME:					240

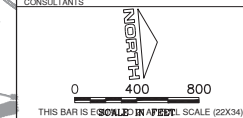
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CMT PROJECT NO: 22004559-00
CAD DWG FILE: 22004559-G-100.DWG
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SHEET TITLE
CONSTRUCTION SAFETY PHASING PLAN - PHASE 1
G-104
SHEET 8 OF 81

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CONSULTANTS



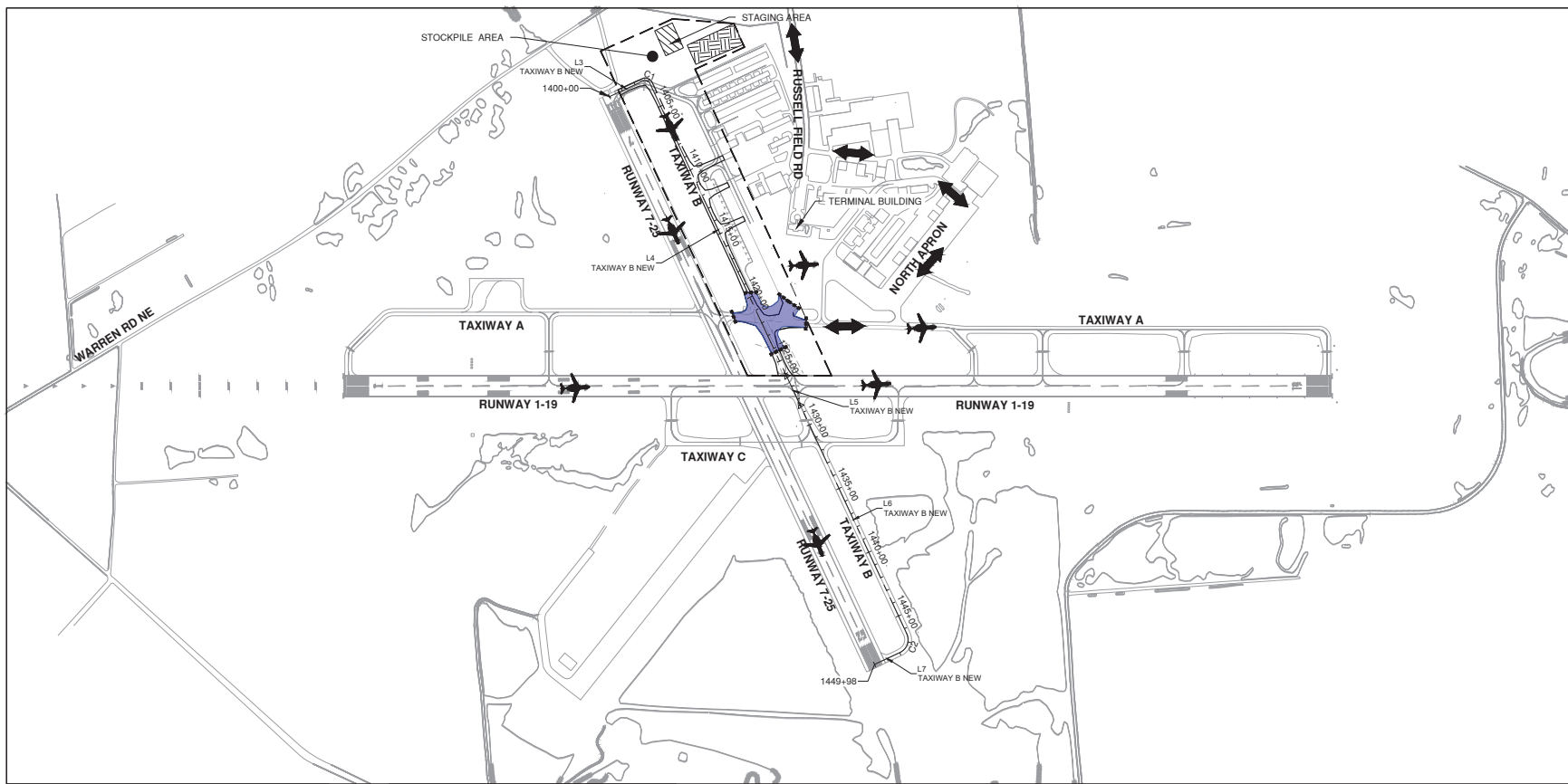
ISSUED FOR BID

TAXIWAY BRAVO WEST
RELOCATION

OWNER



RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA



LEGEND

STAGING AREA	
STOCKPILE AREA	
PROJECT BOUNDARY	
BARRICADE	
RUNWAY CLOSURE MARKER	
AIRCRAFT ACCESS AREA	

BARRICADE SCHEDULE

LOCATION	# OF BARRICADES REQUIRED FOR CONTINUOUS SECTION
RUNWAY 7-25	17
RUNWAY 1-19	25
TAXIWAY A	8
TAXIWAY B	6
TAXILANE	3

PHASE	CONSTRUCTION	AFFECTED OPERATIONS AREAS	WORK HOURS	SAFETY AND SECURITY	CONSTRUCTION TIME (DAYS)
1	MOBILIZATION, EROSION CONTROL SETUP, TAXIWAY BRAVO WEST PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, LIGHTING INSTALLATION.	RUNWAY 7-25 CLOSED RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	170
2	TAXIWAY ALPHA AND BRAVO INTERSECTION PAVEMENT REMOVAL, GRADING, LIGHTING INSTALLATION & REMOVAL, AND TEMPORARY MARKING.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	30
3	TAXIWAY BRAVO PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, LIGHTING INSTALLATION & REMOVAL, AND TEMPORARY MARKING WITHIN RUNWAY 1-19 RSA.	RUNWAY 7-25 OPEN RUNWAY 1-19 CLOSED	NORMAL / UNRESTRICTED INCLUDING THE WEEKEND	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	10
4	TAXILANE CONNECTORS TO BRAVO WEST PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, AND TEMPORARY MARKING.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	15
5	FULL RATE PAVEMENT MARKING (30 DAYS AFTER ASPHALT PAVING), FINAL GRASSING, REMOVE TEMPORARY EROSION CONTROL DEMOLITION.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	15
TOTAL CONSTRUCTION TIME:					240

MARK DATE DESCRIPTION

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SHEET TITLE

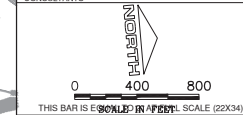
**CONSTRUCTION
SAFETY PHASING
PLAN - PHASE 2**

G-105

SHEET 9 OF 81

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CONSULTANTS



ISSUED FOR BID
TAXIWAY BRAVO WEST
RELOCATION

OWNER



RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA



LEGEND

STAGING AREA	
STOCKPILE AREA	
MILLING STOCKPILE AREA	
PROJECT BOUNDARY	
BARRICADE	
RUNWAY CLOSURE MARKER	
AIRCRAFT ACCESS AREA	

BARRICADE SCHEDULE

LOCATION	# OF BARRICADES REQUIRED FOR CONTINUOUS SECTION
RUNWAY 7-25	17
RUNWAY 1-19	25
TAXIWAY A	8
TAXIWAY B	6
TAXILANE	3

PHASE	CONSTRUCTION	AFFECTED OPERATIONS AREAS	WORK HOURS	SAFETY AND SECURITY	CONSTRUCTION TIME (DAYS)
1	MOBILIZATION, EROSION CONTROL SETUP, TAXIWAY BRAVO WEST PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, LIGHTING INSTALLATION.	RUNWAY 7-25 CLOSED RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	170
2	TAXIWAY ALPHA AND BRAVO INTERSECTION PAVEMENT REMOVAL, GRADING, LIGHTING INSTALLATION & REMOVAL, AND TEMPORARY MARKING.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	30
3	TAXIWAY BRAVO PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, LIGHTING INSTALLATION & REMOVAL, AND TEMPORARY MARKING WITHIN RUNWAY 1-19 RSA.	RUNWAY 7-25 OPEN RUNWAY 1-19 CLOSED	NORMAL / UNRESTRICTED INCLUDING THE WEEKEND	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	10
4	TAXILANE CONNECTORS TO BRAVO WEST PAVEMENT REMOVAL, GRADING, PAVEMENT CONSTRUCTION, AND TEMPORARY MARKING.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	15
5	FULL RATE PAVEMENT MARKING (30 DAYS AFTER ASPHALT PAVING), FINAL GRASSING, REMOVE TEMPORARY EROSION CONTROL DEMOBILIZATION.	RUNWAY 7-25 OPEN RUNWAY 1-19 OPEN	NORMAL / UNRESTRICTED	PLACE BARRICADES AND CLOSURE MARKERS AS SHOWN. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT LEAST 72 HOURS IN ADVANCE OF WORK SO THAT THE AIRPORT MANAGEMENT CAN ISSUE A NOTAM OF CONSTRUCTION. THE MAXIMUM EQUIPMENT HEIGHT IS 35' AGL.	15
TOTAL CONSTRUCTION TIME:					240

MARK	DATE	DESCRIPTION

CMT PROJECT NO: 22004559-00
CAD DWG FILE: 22004559-G-100.DWG
DESIGNED BY: JBH/JMM
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SHEET TITLE
CONSTRUCTION SAFETY PHASING PLAN - PHASE 3
G-106
SHEET 10 OF 81

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CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)



ISSUED FOR BID

TAXIWAY BRAVO WEST
RELOCATION

OWNER



RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

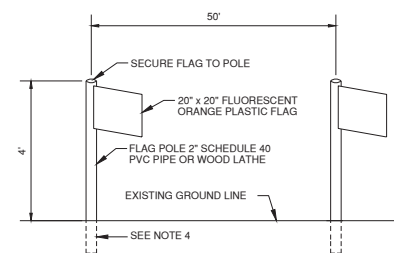
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SHEET TITLE

**CONSTRUCTION
SAFETY PHASING
DETAILS**

G-501
SHEET 12 OF 81



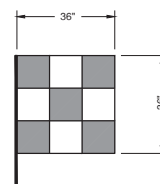
5 FLAG LINE DETAIL
NOT TO SCALE

- NOTES:**
- SPACE FLAG POLES ON 50 FOOT CENTERS.
 - THE CONTRACTOR SHALL ERRECT THE FLAG LINE AT THE LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
 - THE CONTRACTOR SHALL SECURE THE FLAG POLES SO THAT THE TOP OF THE FLAG IS 4 FEET ABOVE THE GROUND.
 - THE FLAG POLES SHALL BE ADEQUATELY SECURED TO WITHSTAND JETBLAST AND REMAIN ERECT AND STABLE.
 - INSPECT FLAGS DAILY. REPAIR OR REPLACE DAMAGED OR WORN FLAGS AND POLES BY THE START OF THE NEXT DAY'S WORK ACTIVITIES.
 - NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR FLAG LINE. PAYMENTS WILL BE MADE UNDER THE UNIT PRICE FOR MOBILIZATION (SPECIFICATION SECTION C-105).



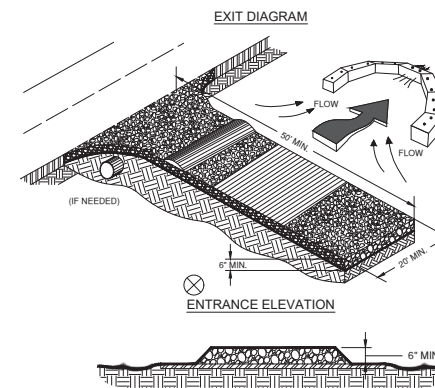
3 TAXIWAY CLOSURE MARKER
NOT TO SCALE

- NOTES:**
- TAXIWAY CLOSURE MARKER SHALL MEET THE REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5340-1M, STANDARDS FOR AIRPORT MARKINGS. THE CONTRACTOR SHALL PROVIDE TWO (2) MARKERS FOR USE ON THIS PROJECT
 - MARKER SHALL BE PLACED ON THE TAXIWAY CENTERLINE AT THE ENTRANCE OF CLOSED TAXIWAY AREA. CONTRACTOR SHALL COORDINATE PLACEMENT WITH THE AIRPORT
 - THE CONTRACTOR SHALL TRANSPORT, POSITION, AND MAINTAIN (IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS) THE MARKERS DURING THE PROJECT
 - NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR TAXIWAY CLOSURE MARKER. PAYMENTS WILL BE MADE UNDER THE UNIT PRICE FOR MOBILIZATION (SPECIFICATION SECTION C-105)
 - CONTRACTOR SHALL ANCHOR FABRIC MARKERS TO PREVENT MOVEMENT BY WIND OR JET BLAST.



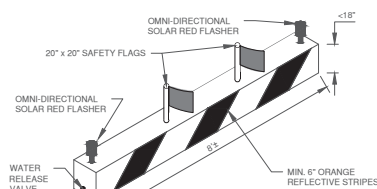
4 EQUIPMENT & VEHICLE SIGNAL FLAG
NOT TO SCALE

- NOTES:**
- VEHICULAR MARKINGS:** CONTRACTOR VEHICLES AND EQUIPMENT SHALL BE MARKED WITH CHECKERED FLAGS AND LIGHTED WITH FLASHING BEACONS TO COMPLY WITH REQUIREMENTS OF FAA AC 150/5210-50.
 - A) ALL CONSTRUCTION EQUIPMENT, SUCH AS DOZERS, BACKHOES, GRADERS, END LOADERS, PANS, OFF-ROAD ARTICULATED DUMP TRUCKS, TRADITIONAL DUMP TRUCKS STATIONED ON-SITE, PAVING TRAIN EQUIPMENT, MILLING EQUIPMENT, AND SIMILAR EQUIPMENT, SHALL BE MARKED WITH AN ORANGE/WHITE CHECKERED FLAG ATTACHED TO THE HIGHEST STATIONARY PART OF THE PIECE OF EQUIPMENT.
 - B) ALL OTHER CONSTRUCTION-RELATED VEHICLES, SUCH AS SUPERVISOR'S VEHICLES, EQUIPMENT MAINTENANCE AND SUPPORT VEHICLES (STEP VANS, ONE-TON PICK-UPS, ETC.), DUMP TRUCKS THAT TRANSPORT MATERIALS BETWEEN OFF-SITE AND ON-SITE LOCATIONS, AND SIMILAR EQUIPMENT, SHALL BE LIGHTED WITH A YELLOW FLASHING BEACON THAT IS VISIBLE DURING TIMES OF DARKNESS, LOW VISIBILITY CONDITIONS AND DAYLIGHT CONDITIONS AND MARKED WITH PERMANENT OR MAGNETIC PLACARDS IDENTIFYING THE COMPANY NAME ON BOTH SIDES OF THE VEHICLE.
 - ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
 - WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
 - THE FLAG SHALL BE A 3 FOOT SQUARE HAVING A CHECKERED PATTERN OF ORANGE AND WHITE SQUARES AT LEAST 1 FOOT ON EACH SIDE.
 - CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.
 - NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR TRUCK SIGNAL FLAGS OR FLASHING BEACONS. PAYMENT WILL BE MADE UNDER THE UNIT PRICE FOR MOBILIZATION (SPECIFICATION SECTION C-105).



6 CONSTRUCTION ENTRANCE DETAIL
NOT TO SCALE

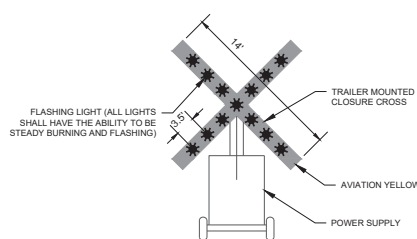
- NOTES:**
- AVOID LOCATING ON STEEP SLOPES OR AT CURVES ON PUBLIC ROADS.
 - REMOVE ALL VEGETATION AND OTHER UNSUITABLE MATERIAL FROM THE FOUNDATION AREA, GRADE, AND CROWN FOR POSITIVE DRAINAGE.
 - AGGREGATE SIZE SHALL BE IN ACCORDANCE WITH NATIONAL STONE ASSOCIATION R-2 (1.5"-3.5" STONE).
 - GRAVEL PAD SHALL HAVE A MINIMUM THICKNESS OF 6".
 - PAD WIDTH SHALL BE EQUAL FULL WIDTH AT ALL POINTS OF VEHICULAR EGRESS, BUT NO LESS THAN 20'.
 - A DIVERSION RIDGE SHOULD BE CONSTRUCTED WHEN GRADE TOWARD PAVED AREA IS GREATER THAN 2%.
 - INSTALL PIPE UNDER THE ENTRANCE IF NEEDED TO MAINTAIN DRAINAGE DITCHES.
 - WHEN WASHING IS REQUIRED, IT SHOULD BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN (DIVERT ALL SURFACE RUNOFF AND DRAINAGE FROM THE ENTRANCE TO A SEDIMENT CONTROL DEVICE).
 - WASHRACKS AND/OR TIRE WASHERS MAY BE REQUIRED DEPENDING ON SCALE AND CIRCUMSTANCE. IF NECESSARY, WASHRACK DESIGN MAY CONSIST OF ANY MATERIAL SUITABLE FOR TRUCK TRAFFIC THAT REMOVE MUD AND DIRT.
 - MAINTAIN AREA IN A WAY THAT PREVENTS TRACKING AND/OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
 - NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR CRUSHED STONE CONSTRUCTION ENT. PAYMENT WILL BE MADE UNDER THE UNIT PRICE FOR MOBILIZATION (SPECIFICATION SECTION C-105).



1 LOW-LEVEL LIGHTED BARRICADE DETAIL
NOT TO SCALE

BARRICADE NOTES:

- FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- PROVIDE OMNI-DIRECTIONAL LIGHTS (2 PER BARRICADE) AND 20" X 20" SAFETY FLAGS (2 PER BARRICADE).
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- BARRICADES MUST BE WATER OR SAND FILLED PER MANUFACTURERS RECOMMENDATIONS. USE OF SAND OR ROCK BAGS IS NOT AN ACCEPTED MEANS TO ANCHOR THESE DEVICES.
- BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING, REPLACING, AND DISPOSING OF BATTERIES. OLD BATTERIES SHALL BE DISPOSED OFF AIRPORT PROPERTY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION AND REMOVAL OF BARRICADES ON AIRPORT PROPERTY.
- NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR BARRICADES. PAYMENTS WILL BE MADE UNDER THE UNIT PRICE FOR MOBILIZATION (SPECIFICATION SECTION C-105).



2 LIGHTED RUNWAY CLOSURE MARKER
NOT TO SCALE

- NOTES:**
- LIGHTED RUNWAY CLOSURE MARKER SHALL MEET THE REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5345-55A, SPECIFICATION FOR L-893 LIGHTED VISUAL TO INDICATE TEMPORARY RUNWAY CLOSURE
 - THE CONTRACTOR SHALL PROVIDE FOUR (4) MARKERS FOR USE ON THIS PROJECT
 - MARKER SHALL BE PLACED ON THE RUNWAY CENTERLINE WITHIN 250 FEET OF THE RUNWAY END. CONTRACTOR SHALL COORDINATE PLACEMENT WITH THE AIRPORT
 - THE CONTRACTOR SHALL TRANSPORT, POSITION, OPERATE, FUEL, REPLACE BULBS (IF REQUIRED), AND MAINTAIN (IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS) THE MARKERS DURING THE PROJECT
 - NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR LIGHTED RUNWAY CLOSURE MARKER. PAYMENTS WILL BE MADE UNDER THE UNIT PRICE FOR MOBILIZATION (SPECIFICATION SECTION C-105).



ISSUED FOR BID
TAXIWAY BRAVO WEST
RELOCATION

OWNER

Richard B. Russell
Regional Airport
FLOYD COUNTY, GA

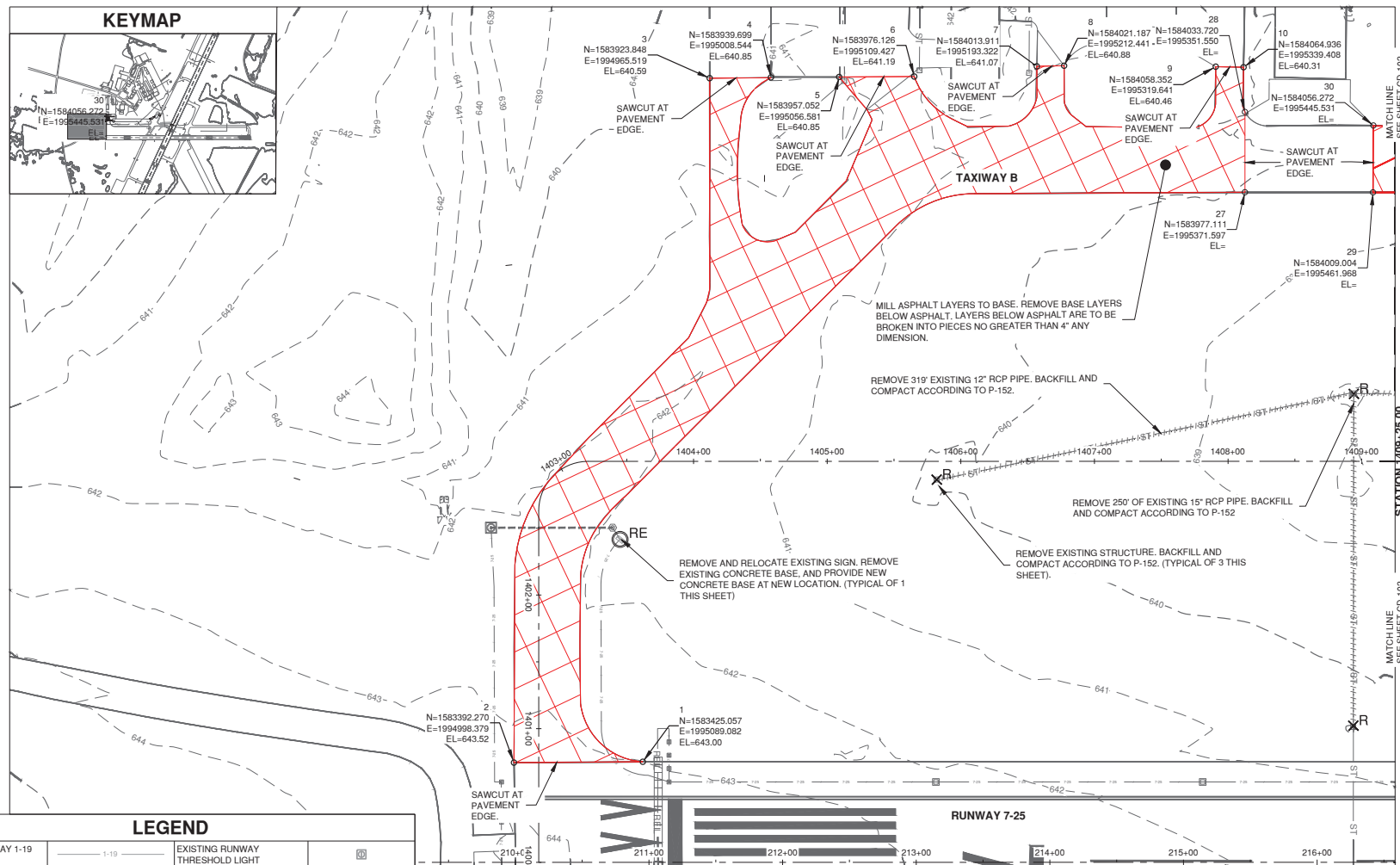
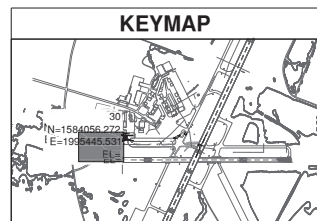
RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

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SHEET TITLE
**EXISTING CONDITIONS
AND DEMOLITION -
SHEET 1**

CD-101
SHEET 13 OF 81



LEGEND

EXISTING RUNWAY 1-19 CIRCUIT	1-19	EXISTING RUNWAY THRESHOLD LIGHT	⊠
EXISTING RUNWAY 7-25 CIRCUIT	7-25	EXISTING TAXIWAY GUIDANCE SIGN	⊠
EXISTING TAXIWAY A CIRCUIT	TXY-A	EXISTING TAXIWAY EDGE LIGHT	⊠
EXISTING WINDCONE CIRCUIT	WIND	REMOVE CABLE/ REMOVE PIPE	-----
EXISTING NAVIGATIONAL AID CIRCUITS	PAPI REIL AWOS	REMOVE STRUCTURE FOR LOCATION ON NEW BASE	○RE
EXISTING DUCT BANK	-----	REMOVE STRUCTURE	XR
EXISTING RUNWAY EDGE LIGHT	⊠	MILL PAVEMENT	▨
EXISTING HANDHOLE (ELECTRIC)	⊠	REMOVE PAVEMENT - FULL DEPTH MILLING	⊠
EXISTING STORM SEWER	ST	EXISTING CONTOURS	- - - -
EXISTING HEADWALL & CATCH BASIN	⊠		

NOTES

- EXISTING LIGHTS, GUIDANCE SIGNS, AND AIRFIELD CIRCUITS, OTHER THAN THOSE INDICATED FOR REMOVAL, ARE TO BE PROTECTED FROM DAMAGE. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT THEIR EXPENSE.
- LIGHT FIXTURES, LAMPS, TRANSFORMERS, SIGNS AND ELEVATED MARKERS TO BE REMOVED SHALL BE TURNED OVER TO AIRPORT MAINTENANCE. IF AIRPORT MAINTENANCE DOES NOT WANT ANY OF THE REMOVED MATERIALS THEN THE CONTRACTOR SHALL DISPOSE OF OFF AIRPORT PROPERTY AT NO ADDITIONAL COST TO THE CONTRACT. LIGHT BASES AND SIGN FOUNDATIONS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
- WHEN CABLES INSTALLED IN CONDUIT CONTRACTOR SHALL REMOVE ABANDONED CABLE FROM EXISTING CONDUIT.
- PRIOR TO REMOVAL OF AIRFIELD SPLICE CAN, CONTRACTOR SHALL VERIFY THAT ALL ELECTRICAL CIRCUITS CONTAINED IN THE SPLICE CAN HAVE BEEN ABANDONED.
- ALL EXISTING AIRFIELD LIGHTING CIRCUITS CONNECTED TO SIGNS AND WINDCONES TO REMAIN, SHALL BE REMOVED AND REPLACED WITH NEW CIRCUIT AS SHOWN ON THE PLANS.
- CONTRACTOR SHALL COORDINATE WITH FAA TO LOCATE EXISTING FAA MALSR, AWOS AND PAPI CIRCUITS PRIOR TO ANY CONSTRUCTION ACTIVITY. EXISTING MALSR, AWOS AND PAPI CIRCUITS AND EQUIPMENT SHALL BE PROTECTED DURING CONSTRUCTION
- CONTRACTOR SHALL FIELD INVESTIGATE AND VERIFY EXISTING CIRCUIT ROUTING. INSTALL JUMPERS BETWEEN EXISTING LIGHTS TO KEEP EXISTING CIRCUITS OPERATIONAL FOR EACH CONSTRUCTION PHASE AS LISTED ON PHASING SHEETS. THE COST OF ALL TEMPORARY CONNECTIONS SHALL BE INCIDENTAL TO THE CONTRACT.
- THE LOCATION OF ALL UTILITIES SHOWN ON THESE PLANS ARE TAKEN FROM THE BEST AVAILABLE RECORDS. THE ENGINEER AND AIRPORT SHALL NOT BE RESPONSIBLE FOR ANY OMISSION OR VARIATION FROM THE LOCATIONS SHOWN. THE CONTRACTOR SHALL CONTACT THE AIRPORT MAINTENANCE DEPARTMENT, FAA AIRWAY FACILITIES AND OTHER UTILITY LOCATIONS REQUIRED TO IDENTIFY AND LOCATE EXISTING UTILITIES AT THEIR OWN EXPENSE.
- UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS AND, THEREFORE, THEIR LOCATIONS SHALL BE CONSIDERED APPROXIMATE ONLY. THERE MAY BE OTHER UTILITIES, THE EXISTENCE OF WHICH IS AT PRESENT NOT KNOWN. VERIFICATION OF THE LOCATIONS OF THE UNDERGROUND UTILITIES SHOWN OR NOT SHOWN SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR SHALL PROTECT ALL RUNWAY AND TAXIWAY MARKING OUTSIDE OF THE PROJECT WORK AREA. ANY NECESSARY CLEANING AND REMARKING WILL BE COMPLETED AT THE CONTRACTOR'S EXPENSE.

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TAXIWAY BRAVO WEST
RELOCATION



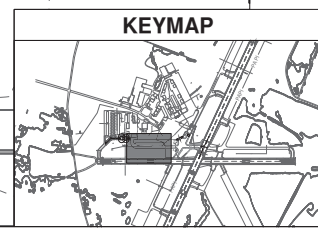
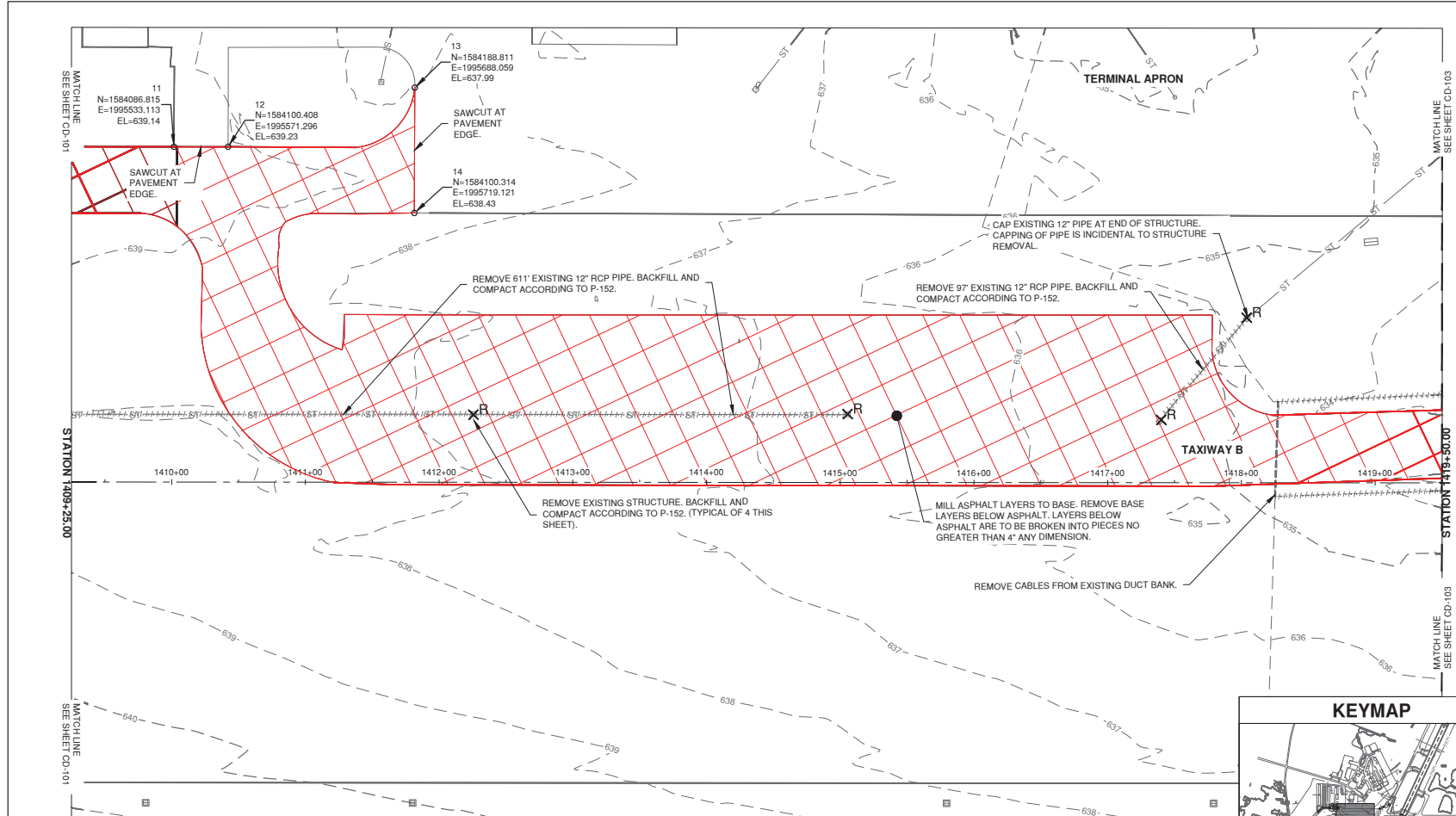
RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

MARK	DATE	DESCRIPTION

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CAD DWG FILE:	22004559-CD-1.DWG
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SHEET TITLE
**EXISTING CONDITIONS
AND DEMOLITION -
SHEET 2**

CD-102
SHEET 14 OF 81



LEGEND			
EXISTING RUNWAY 1-19 CIRCUIT	1-19	EXISTING RUNWAY THRESHOLD LIGHT	⊠
EXISTING RUNWAY 7-25 CIRCUIT	7-25	EXISTING TAXIWAY GUIDANCE SIGN	▬
EXISTING TAXIWAY A CIRCUIT	TXY-A	EXISTING TAXIWAY EDGE LIGHT	⊠
EXISTING WINDCONE CIRCUIT	WIND	REMOVE CABLE/ REMOVE PIPE	-----
EXISTING NAVIGATIONAL AID CIRCUITS	PAPI REIL AWCS	REMOVE STRUCTURE FOR LOCATION ON NEW BASE	⊙RE
EXISTING DUCT BANK	-----	REMOVE STRUCTURE	XR
EXISTING RUNWAY EDGE LIGHT	⊠	MILL PAVEMENT	▨
EXISTING HANDHOLE (ELECTRIC)	⊕	REMOVE PAVEMENT - FULL DEPTH MILLING	⊠
EXISTING STORM SEWER	—ST—	EXISTING CONTOURS	-----
EXISTING HEADWALL & CATCH BASIN	⊠		

NOTES

- EXISTING LIGHTS, GUIDANCE SIGNS, AND AIRFIELD CIRCUITS, OTHER THAN THOSE INDICATED FOR REMOVAL, ARE TO BE PROTECTED FROM DAMAGE. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT THEIR EXPENSE.
- LIGHT FIXTURES, LAMPS, TRANSFORMERS, SIGNS AND ELEVATED MARKERS TO BE REMOVED SHALL BE TURNED OVER TO AIRPORT MAINTENANCE. IF AIRPORT MAINTENANCE DOES NOT WANT ANY OF THE REMOVED MATERIALS THEN THE CONTRACTOR SHALL DISPOSE OF OFF AIRPORT PROPERTY AT NO ADDITIONAL COST TO THE CONTRACT. LIGHT BASES AND SIGN FOUNDATIONS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
- WHEN CABLES INSTALLED IN CONDUIT CONTRACTOR SHALL REMOVE ABANDONED CABLE FROM EXISTING CONDUIT.
- PRIOR TO REMOVAL OF AIRFIELD SPLICE CAN, CONTRACTOR SHALL VERIFY THAT ALL ELECTRICAL CIRCUITS CONTAINED IN THE SPLICE CAN HAVE BEEN ABANDONED.
- ALL EXISTING AIRFIELD LIGHTING CIRCUITS CONNECTED TO SIGNS AND WINDCONES TO REMAIN, SHALL BE REMOVED AND REPLACED WITH NEW CIRCUIT AS SHOWN ON THE PLANS.
- CONTRACTOR SHALL COORDINATE WITH FAA TO LOCATE EXISTING FAA MALSR, AWOS AND PAPI CIRCUITS PRIOR TO ANY CONSTRUCTION ACTIVITY. EXISTING MALSR, AWOS AND PAPI CIRCUITS AND EQUIPMENT SHALL BE PROTECTED DURING CONSTRUCTION.
- CONTRACTOR SHALL FIELD INVESTIGATE AND VERIFY EXISTING CIRCUIT ROUTING, INSTALL JUMPERS BETWEEN EXISTING LIGHTS TO KEEP EXISTING CIRCUITS OPERATIONAL FOR EACH CONSTRUCTION PHASE AS LISTED ON PHASING SHEETS. THE COST OF ALL TEMPORARY CONNECTIONS SHALL BE INCIDENTAL TO THE CONTRACT.
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- CONTRACTOR SHALL PROTECT ALL RUNWAY AND TAXIWAY MARKING OUTSIDE OF THE PROJECT WORK AREA. ANY NECESSARY CLEANING AND REMARKING WILL BE COMPLETED AT THE CONTRACTOR'S EXPENSE.

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CONSULTANTS



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TAXIWAY BRAVO WEST RELOCATION

OWNER



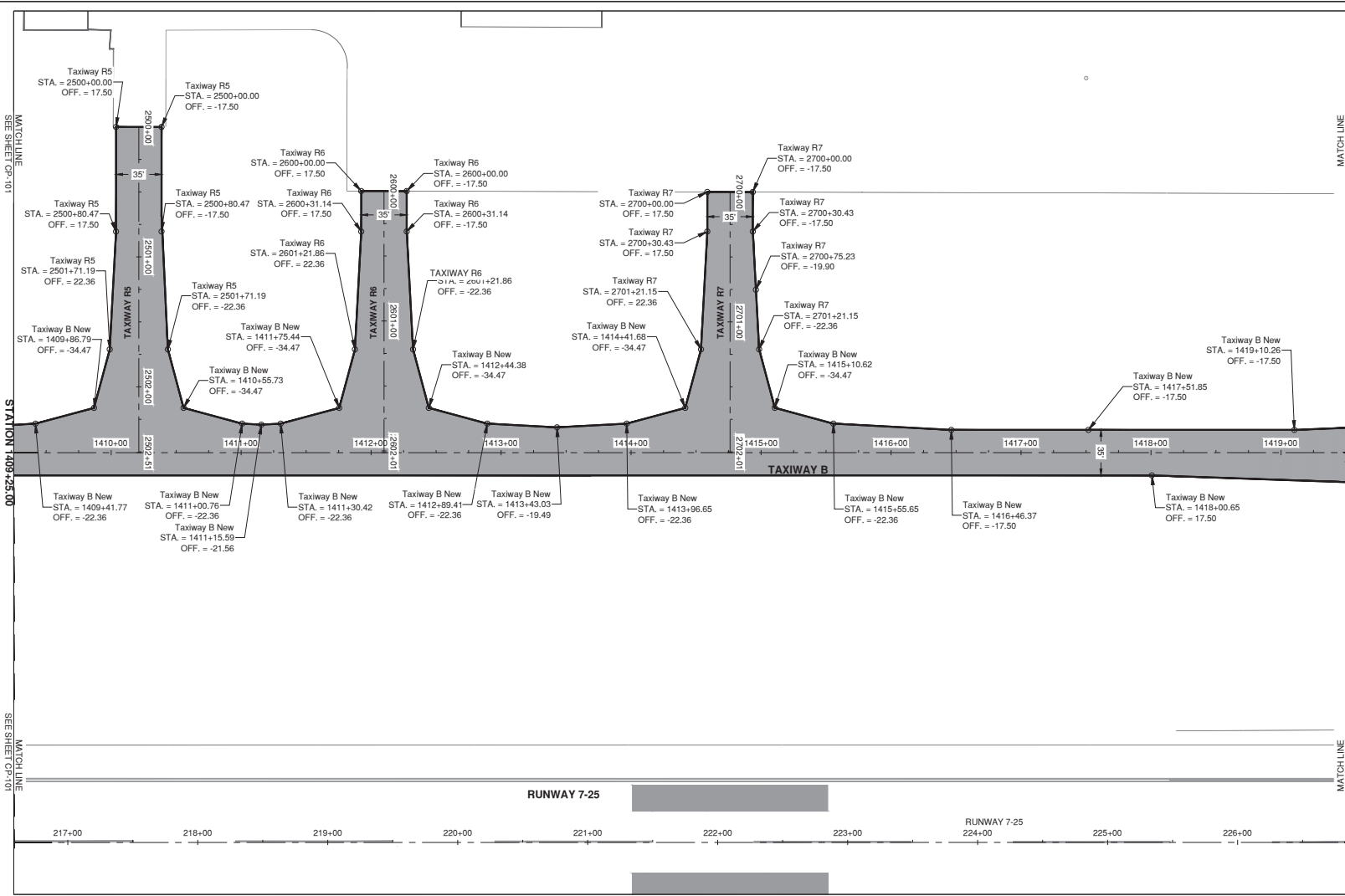
RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

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 CAD DWG FILE: 22004559-CP-1.DWG
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PROPOSED GEOMETRY PLAN - SHEET 2

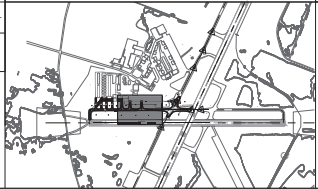
SHEET 18 OF 81



LEGEND

EXISTING PAVEMENT	
PROPOSED PAVEMENT	

KEYMAP



NOTES

- CONTRACTOR SHALL PROTECT ALL RUNWAY AND TAXIWAY MARKING OUTSIDE OF THE PROJECT WORK AREA. ANY NECESSARY CLEANING AND REMARKING WILL BE COMPLETED AT THE CONTRACTOR'S EXPENSE.

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CONSULTANTS



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TAXIWAY BRAVO WEST
RELOCATION

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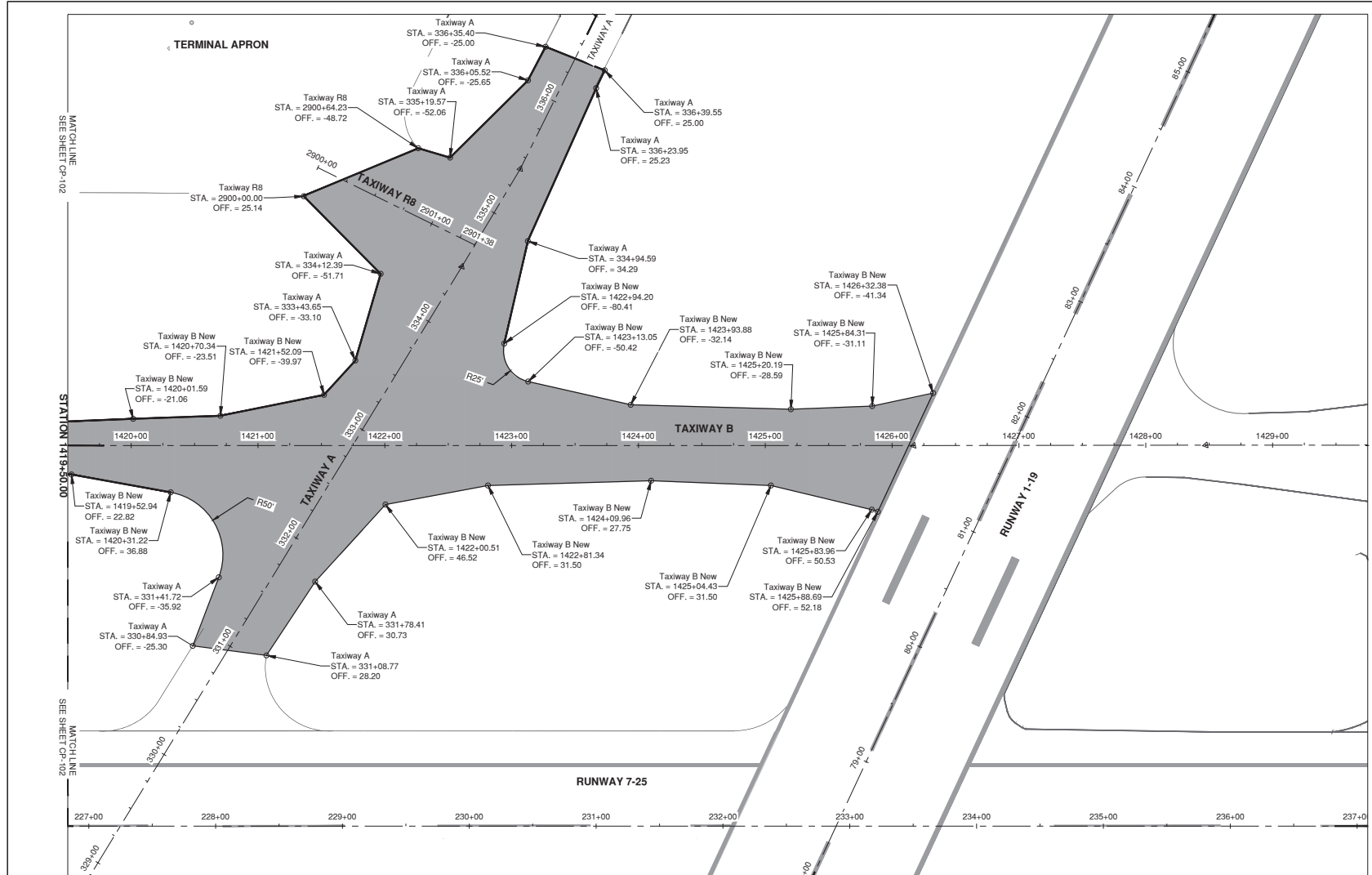
RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

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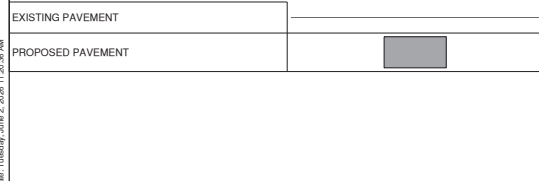
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SHEET TITLE
**PROPOSED
GEOMETRY PLAN -
SHEET 3**

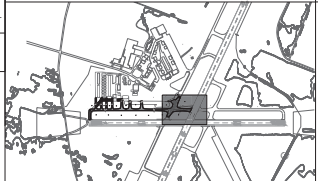
SHEET 19 OF 81



LEGEND



KEYMAP



NOTES

- CONTRACTOR SHALL PROTECT ALL RUNWAY AND TAXIWAY MARKING OUTSIDE OF THE PROJECT WORK AREA. ANY NECESSARY CLEANING AND REMARKING WILL BE COMPLETED AT THE CONTRACTOR'S EXPENSE.

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CONSULTANTS



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**TAXIWAY BRAVO WEST
RELOCATION**

OWNER



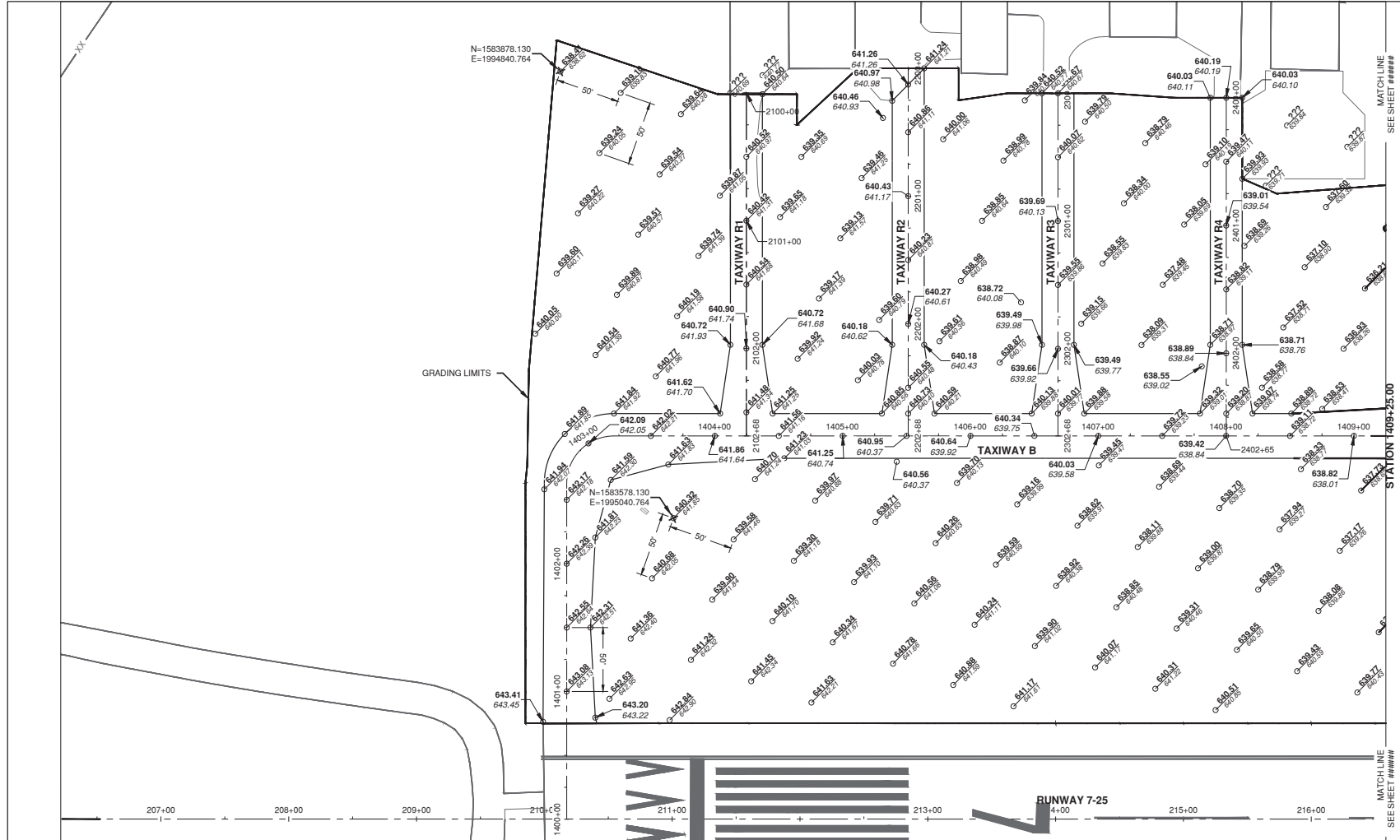
RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

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DRAWN BY:	JBH
CHECKED BY:	JMM
APPROVED BY:	MJR
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SHEET TITLE
**STAKING PLAN -
SHEET 1**

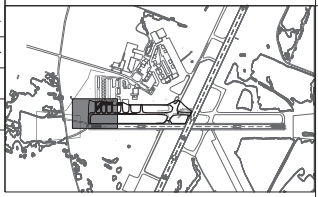
SHEET **20** OF **81**



LEGEND

EXISTING PAVEMENT	
PROPOSED PAVEMENT	
PROPOSED SURFACE ELEVATION	○ XXX.XX
EXISTING GROUND ELEVATION	○ xxx.xx

KEYMAP



NOTES

- AFTER PLACEMENT OF P-209, AND SUBSEQUENT LIFTS OF GDOT-402 ASPHALT COURSE, THE CONTRACTOR SHALL PERFORM A GROUND RUN TOPOGRAPHIC SURVEY OF THE PROPOSED TAXIWAY AND CONNECTING TAXIWAYS USING A TOTAL STATION SURVEY INSTRUMENT. TOPOGRAPHIC MAPPING USING GPS, LIDAR, OR PHOTOGRAMMETRY WILL NOT BE ACCEPTED. THE LIMITS OF THIS SURVEY SHALL EXTEND TO THE EDGE OF PAVEMENT AND ENCOMPASS THE ENTIRE LENGTH AND WIDTH OF THE PAVED TAXIWAY SURFACE AS WELL AS THE PAVED CONNECTING TAXIWAYS.
- SURVEY SHALL BE ACCURATE TO 0.01'. THE SURVEY SHALL BE SHOT AT THE SAME LOCATIONS SHOWN ON THE STAKING PLAN. ANY AREAS WHERE THE CONTRACTOR'S SURVEY VARIES FROM THE EXISTING ELEVATION SHOWN ON THE
- PLANS BY MORE THAN 0.04' SHALL BE HIGHLIGHTED. THE SURVEY SHALL BE PERFORMED BY AND SEALED BY A REGISTERED LAND SURVEYOR IN THE STATE OF GEORGIA. PROVIDE THE COMPLETED SURVEY IN PDF AND CAD FORMAT.
- THE RPR RESERVES THE RIGHT TO ADJUST THE STAKING PLAN BASED ON THE PROVIDED SURVEY DATA. THE CONTRACTOR WILL BE ALLOWED FIVE DAYS TO COMPLETE THIS WORK AND SHALL COORDINATE WITH THE AIRPORT FOR ACCESS TO THE TAXIWAY.
- PROVIDE THE COMPLETED SURVEY TO THE RPR FOR ANALYSIS AT LEAST 5 DAYS IN ADVANCE OF THE PLANNED START OF WORK.
- CONTRACTOR SHALL PROTECT ALL RUNWAY AND TAXIWAY MARKING OUTSIDE OF THE PROJECT WORK AREA. ANY NECESSARY CLEANING AND REMARKING WILL BE COMPLETED AT THE CONTRACTOR'S EXPENSE.
- ELEVATIONS SHOWN WHICH ARE NOT ON PAVEMENT ARE ON A 50'X50' GRID ORIENTED TO TRUE NORTH. ELEVATIONS GIVEN ALONG PAVEMENT CENTERLINES ARE GIVEN AT 50' INTERVALS ASSOCIATED WITH THE RELEVANT ALIGNMENT. ELEVATIONS SHOWN ON PAVEMENT EDGES ARE GIVEN AT GEOMETRY POINTS.

CONSULTANTS



TAXIWAY BRAVO WEST
RELOCATION

OWNER



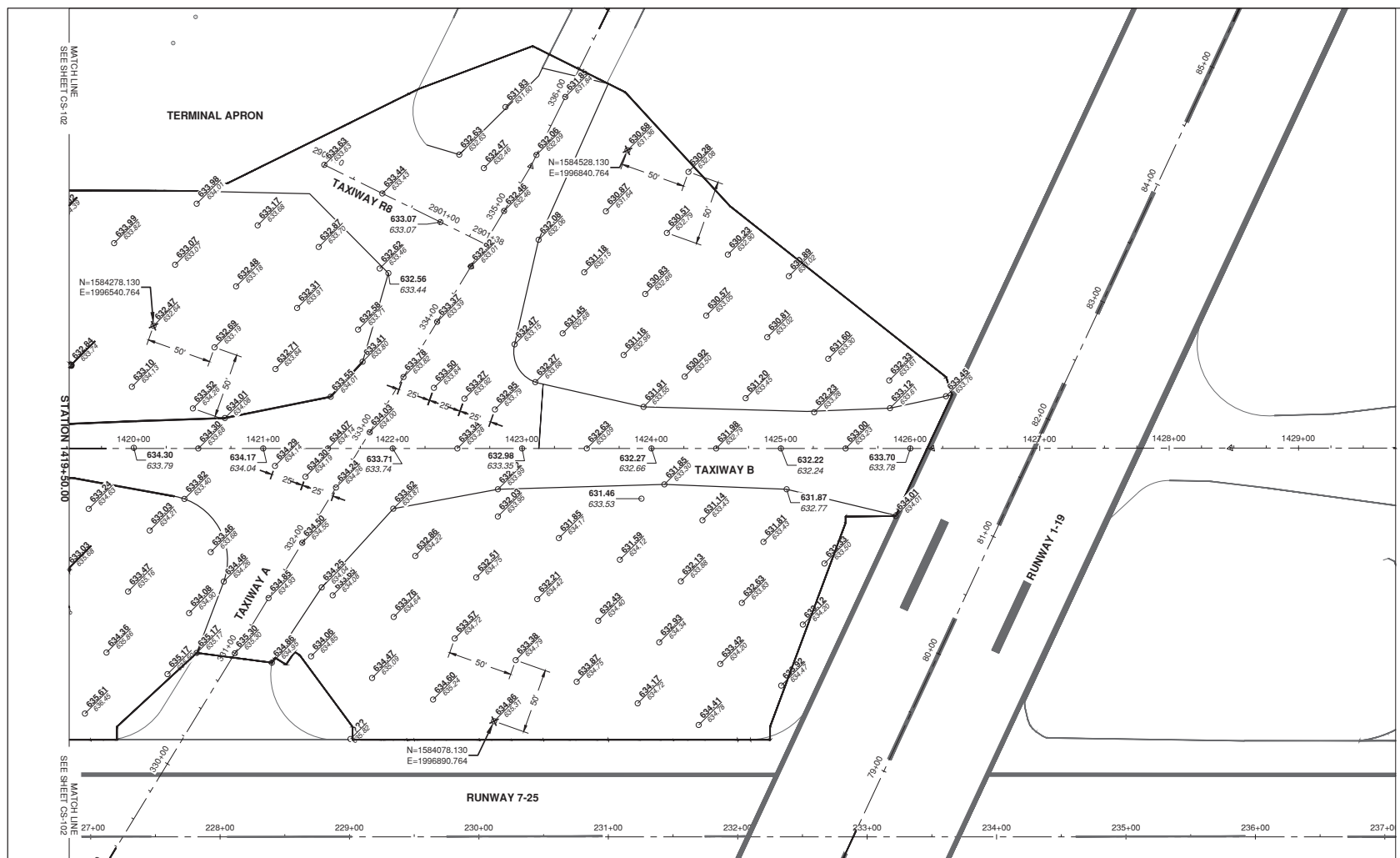
RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

MARK	DATE	DESCRIPTION

CMT PROJECT NO: 22004559-00
CAD DWG FILE: 22004559-CS-1.DWG
DESIGNED BY: JBH/JMM
DRAWN BY: JBH
CHECKED BY: JMM
APPROVED BY: MJR
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SHEET TITLE
**STAKING PLAN -
SHEET 3**

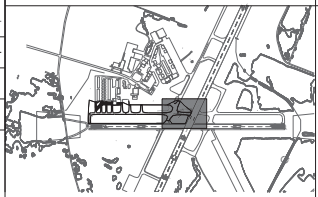
SHEET **22** OF **81**



LEGEND

EXISTING PAVEMENT	
PROPOSED PAVEMENT	
PROPOSED SURFACE ELEVATION	○ XXX.XX
EXISTING GROUND ELEVATION	○ xxx.xx

KEYMAP



NOTES

- AFTER PLACEMENT OF P-209, AND SUBSEQUENT LIFTS OF GDOT-402 ASPHALT COURSE, THE CONTRACTOR SHALL PERFORM A GROUND RUN TOPOGRAPHIC SURVEY OF THE PROPOSED TAXIWAY AND CONNECTING TAXIWAYS USING A TOTAL STATION SURVEY INSTRUMENT. TOPOGRAPHIC MAPPING USING GPS, LIDAR, OR PHOTOGRAMMETRY WILL NOT BE ACCEPTED. THE LIMITS OF THIS SURVEY SHALL EXTEND TO THE EDGE OF PAVEMENT AND ENCOMPASS THE ENTIRE LENGTH AND WIDTH OF THE PAVED TAXIWAY SURFACE AS WELL AS THE PAVED CONNECTING TAXIWAYS.
- SURVEY SHALL BE ACCURATE TO 0.01'. THE SURVEY SHALL BE SHOT AT THE SAME LOCATIONS SHOWN ON THE STAKING PLAN. ANY AREAS WHERE THE CONTRACTOR'S SURVEY VARIES FROM THE EXISTING ELEVATION SHOWN ON THE PLANS BY MORE THAN 0.04' SHALL BE HIGHLIGHTED. THE SURVEY SHALL BE PERFORMED BY AND SEALED BY A REGISTERED LAND SURVEYOR IN THE STATE OF GEORGIA. PROVIDE THE COMPLETED SURVEY IN PDF AND CAD FORMAT.
- THE RPR RESERVES THE RIGHT TO ADJUST THE STAKING PLAN BASED ON THE PROVIDED SURVEY DATA. THE CONTRACTOR WILL BE ALLOWED FIVE DAYS TO COMPLETE THIS WORK AND SHALL COORDINATE WITH THE AIRPORT FOR ACCESS TO THE TAXIWAY.
- PROVIDE THE COMPLETED SURVEY TO THE RPR FOR ANALYSIS AT LEAST 5 DAYS IN ADVANCE OF THE PLANNED START OF WORK.
- CONTRACTOR SHALL PROTECT ALL RUNWAY AND TAXIWAY MARKING OUTSIDE OF THE PROJECT WORK AREA. ANY NECESSARY CLEANING AND REMARKING WILL BE COMPLETED AT THE CONTRACTOR'S EXPENSE.
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 Date: Tuesday, June 2, 2026 11:21:01 AM

CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (22X34)



ISSUED FOR BID
TAXIWAY BRAVO WEST
RELOCATION

OWNER



RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

MARK DATE DESCRIPTION

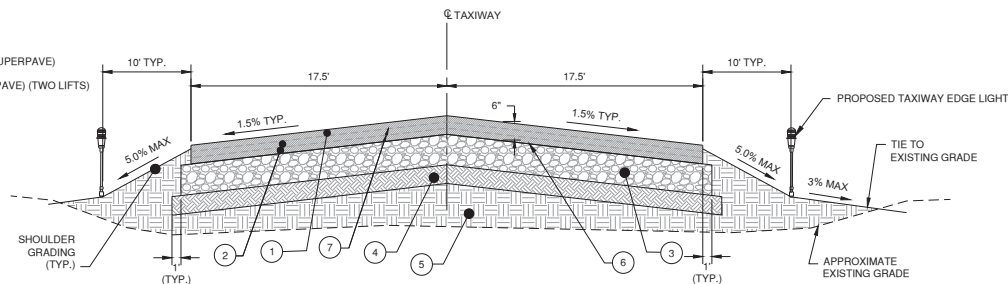
CMT PROJECT NO: 22004559-00
CAD DWG FILE: 22004559-CH6.DWG
DESIGNED BY: JBH
DRAWN BY: JBH
CHECKED BY: JMM
APPROVED BY: MJR
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SHEET TITLE
TYPICAL SECTIONS

CS-501
SHEET 23 OF 81

LEGEND

- ① 2" ASPHALT SURFACE COURSE (GDOT-402-5.01 12.5mm SUPERPAVE)
- ② 2" ASPHALT BASE COURSE (GDOT-402-5.02 19mm SUPERPAVE) (TWO LIFTS)
- ③ 12" GRADED AGGREGATE BASE COURSE (P-209)
- ④ 6" LIME-TREATED SUBGRADE IN ONE LIFT (P-155)
- ⑤ EMBANKMENT IN PLACE - (P-152)
- ⑥ BITUMINOUS PRIME COAT (P-602)
- ⑦ BITUMINOUS TACK COAT (P-603)

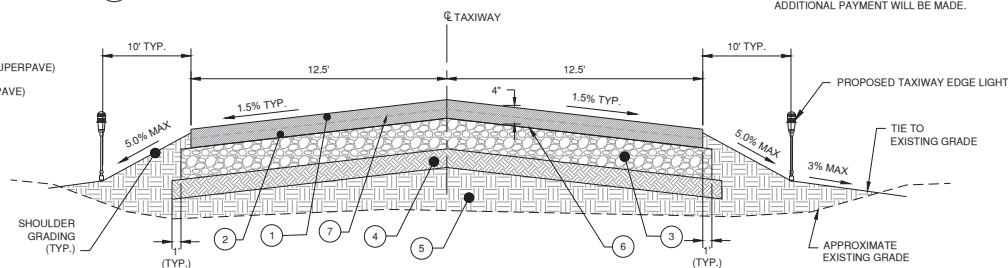


① TYPICAL SECTION
PROPOSED TAXIWAY B STA 1421+45 TO STA 1426+25, A, AND R5-8
NOT TO SCALE

NOTE:
CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING A SMOOTH TIE-IN TO EXISTING RUNWAY 7/25 EDGE FREE OF DIPS AND/OR BUMPS. ANY ADDITIONAL COSTS ASSOCIATED WITH PROVIDING A SMOOTH AND SATISFACTORY TIE-IN INCLUDING BUT NOT LIMITED TO REMOBILIZING, MILLING, SAW CUTTING, ASPHALT REMOVAL AND/OR PLACEMENT, ETC. SHALL BE INCIDENTAL TO THE OVERALL PROJECT COST AND NO ADDITIONAL PAYMENT WILL BE MADE.

LEGEND

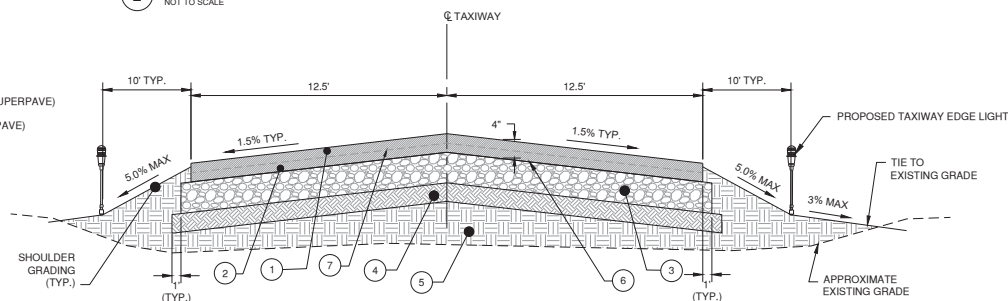
- ① 2" ASPHALT SURFACE COURSE (GDOT-402-5.01 12.5mm SUPERPAVE)
- ② 2" ASPHALT BASE COURSE (GDOT-402-5.02 19mm SUPERPAVE)
- ③ 8" GRADED AGGREGATE BASE COURSE (P-209)
- ④ 6" LIME-TREATED SUBGRADE IN ONE LIFT (P-155)
- ⑤ EMBANKMENT IN PLACE - (P-152)
- ⑥ BITUMINOUS PRIME COAT (P-602)
- ⑦ BITUMINOUS TACK COAT (P-603)



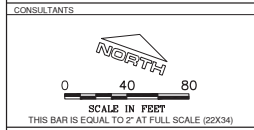
② TYPICAL SECTION
PROPOSED TAXIWAY R5-7, AND TAXIWAY B STA 1400+00 TO STA 1421+45
NOT TO SCALE

LEGEND

- ① 2" ASPHALT SURFACE COURSE (GDOT-402-5.01 12.5mm SUPERPAVE)
- ② 2" ASPHALT BASE COURSE (GDOT-400-5.02 19mm SUPERPAVE)
- ③ 6" GRADED AGGREGATE BASE COURSE (P-209)
- ④ 6" LIME-TREATED SUBGRADE IN ONE LIFT (P-155)
- ⑤ EMBANKMENT IN PLACE - (P-152)
- ⑥ BITUMINOUS PRIME COAT (P-602)
- ⑦ BITUMINOUS TACK COAT (P-603)



③ TYPICAL SECTION
PROPOSED TAXIWAY R1-4
NOT TO SCALE



ISSUED FOR BID
TAXIWAY BRAVO WEST
RELOCATION

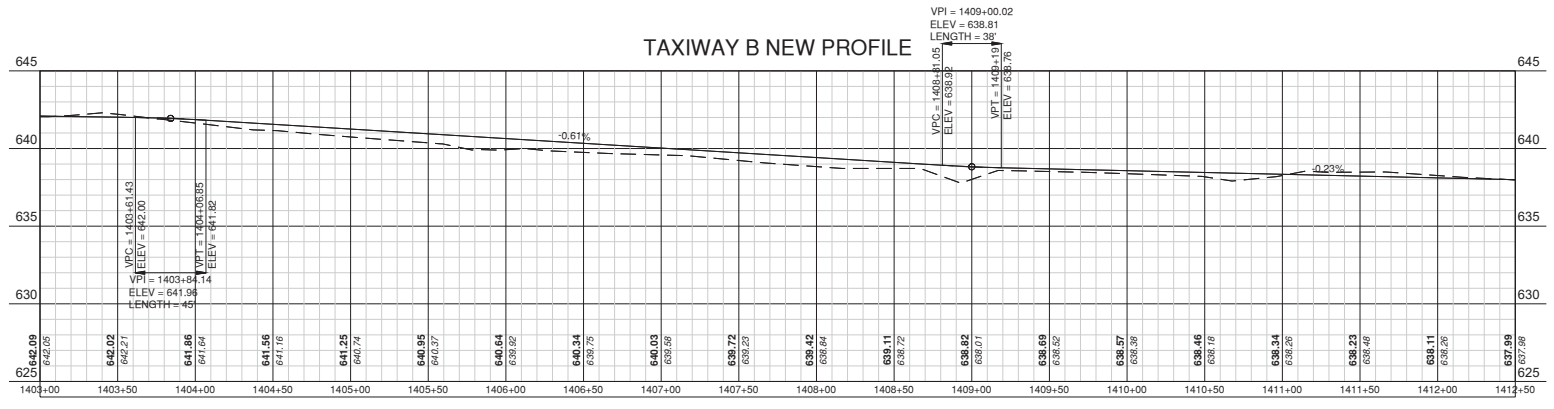
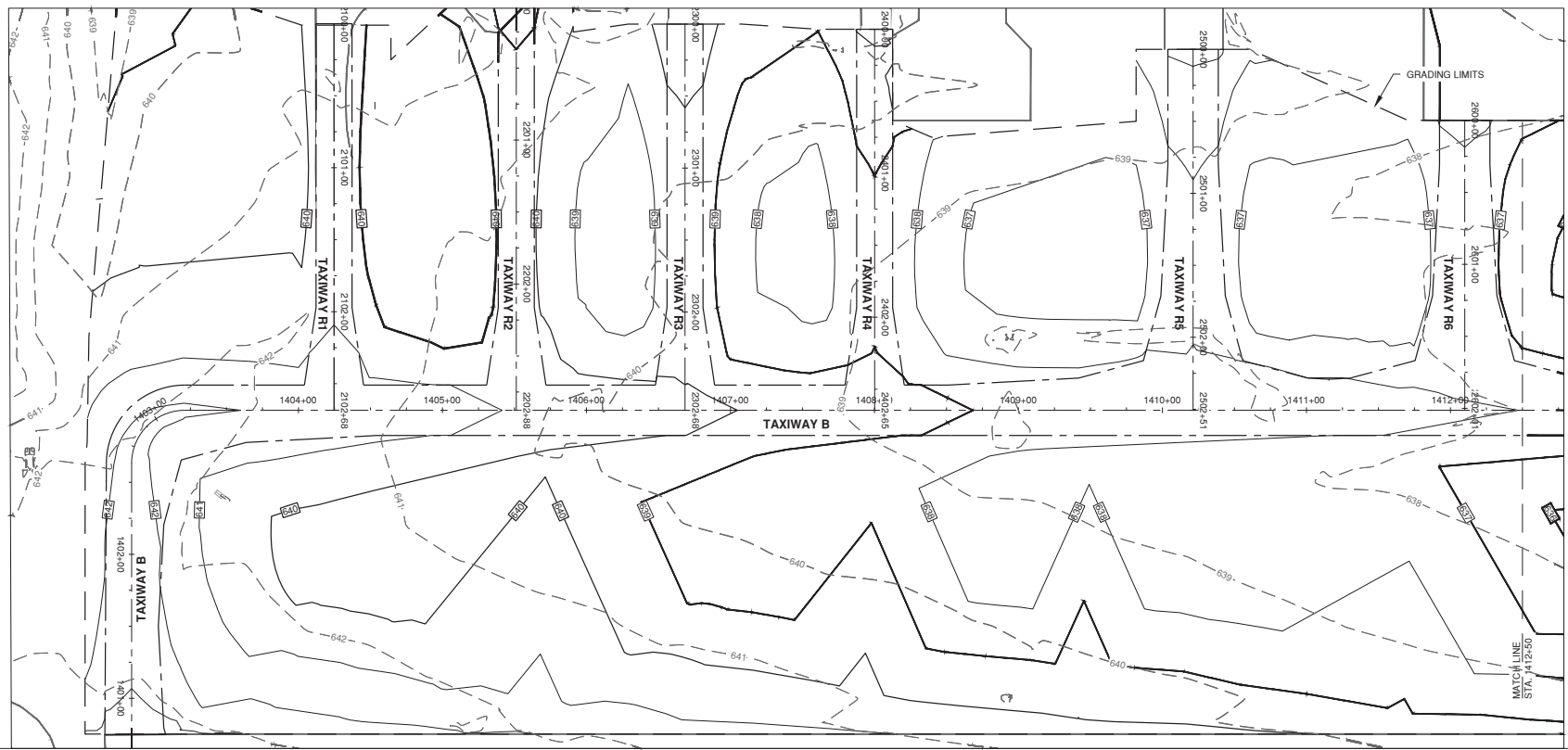


RICHARD B. RUSSELL
REGIONAL AIRPORT
FLOYD COUNTY, GEORGIA

MARK	DATE	DESCRIPTION

CMT PROJECT NO: 22004559-00
CAD DWG FILE: 22004559-CP-2.DWG
DESIGNED BY: JBH/JMM
DRAWN BY: JBH
CHECKED BY: JMM
APPROVED BY: MJR
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SHEET TITLE
PLAN AND PROFILE - TAXIWAY B 2



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